



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN2
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC OPERATIONS DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-1(495)	ROADWAY-SIGN2

REV. 01-09-25: ADDED SHEET.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Anthony Lee Washington III
2024.12.10 21:43:12-06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON III P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC OPERATIONS DRAWINGS	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES.....	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
GENERAL NOTES.....	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES.....	2E, 2E1
TABULATED QUANTITIES.....	2F
RIGHT-OF-WAY NOTES, UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....	4

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-1(495)	ROADWAY-SIGN1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SHELBY COUNTY

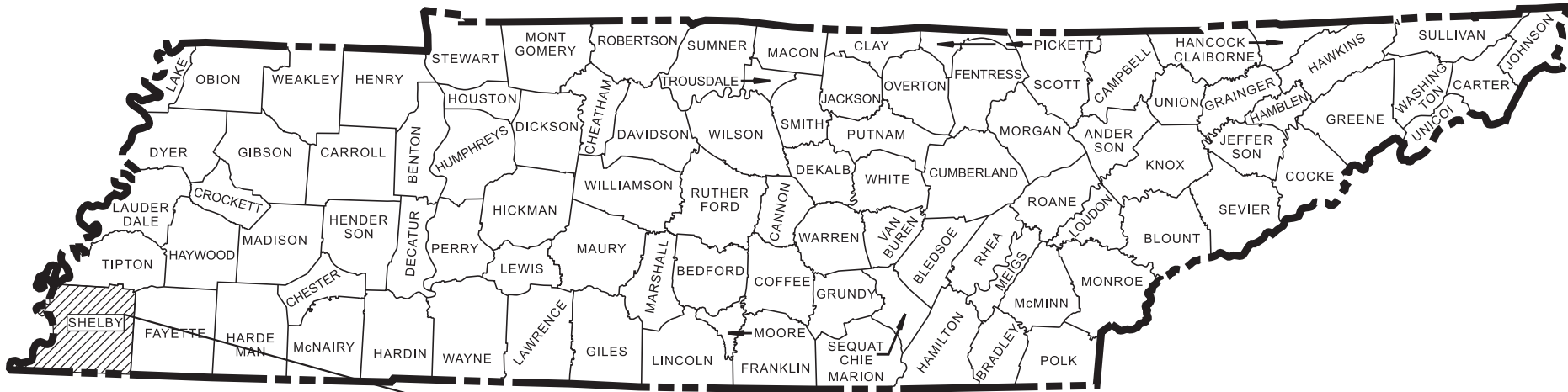
S.R. 1
FROM: L.M. 26.00 (WEST OF LAMB ROAD)
TO: L.M. 30.00 (NEAR FAYETTE COUNTY LINE)

RESURFACE & SAFETY
MILL, 411D, PAVEMENT MARKINGS

STATE HIGHWAY NO. 1 F.A.H.S. NO. 70 / 79

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH/HSIP-1(495)	
STATE PROJ. NO.	79S001-F8-008	
STATE PROJ. NO.	79S001-F3-008	
STATE PROJ. NO.	79S001-M3-011	



PROJECT LOCATION
BRIDGE ID. # 79SR0010029 79SR0010030 79SR0010047
79SR0010049 79SR0010046 79SR0010045
79SR0010031 79SR0010033 79SR0010035
79SR0010037

79S001-F3-008
79S001-F8-008
END PROJECT NO. NH/HSIP-1(495) RESURFACE & SAFETY
L.M. 30.00 (NEAR FAYETTE COUNTY LINE)

BRIDGE-DECK-REPAIR PROJECT NO. 79S001-M3-011
SR1- @ L.M. 26.85

79S001-F8-008
79S001-F3-008
BEGIN PROJECT NO. NH/HSIP-1(495) RESURFACE & SAFETY
L.M. 26.00 (FROM WEST OF LAMB ROAD)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS
AND IN THE PROPOSAL CONTRACT.

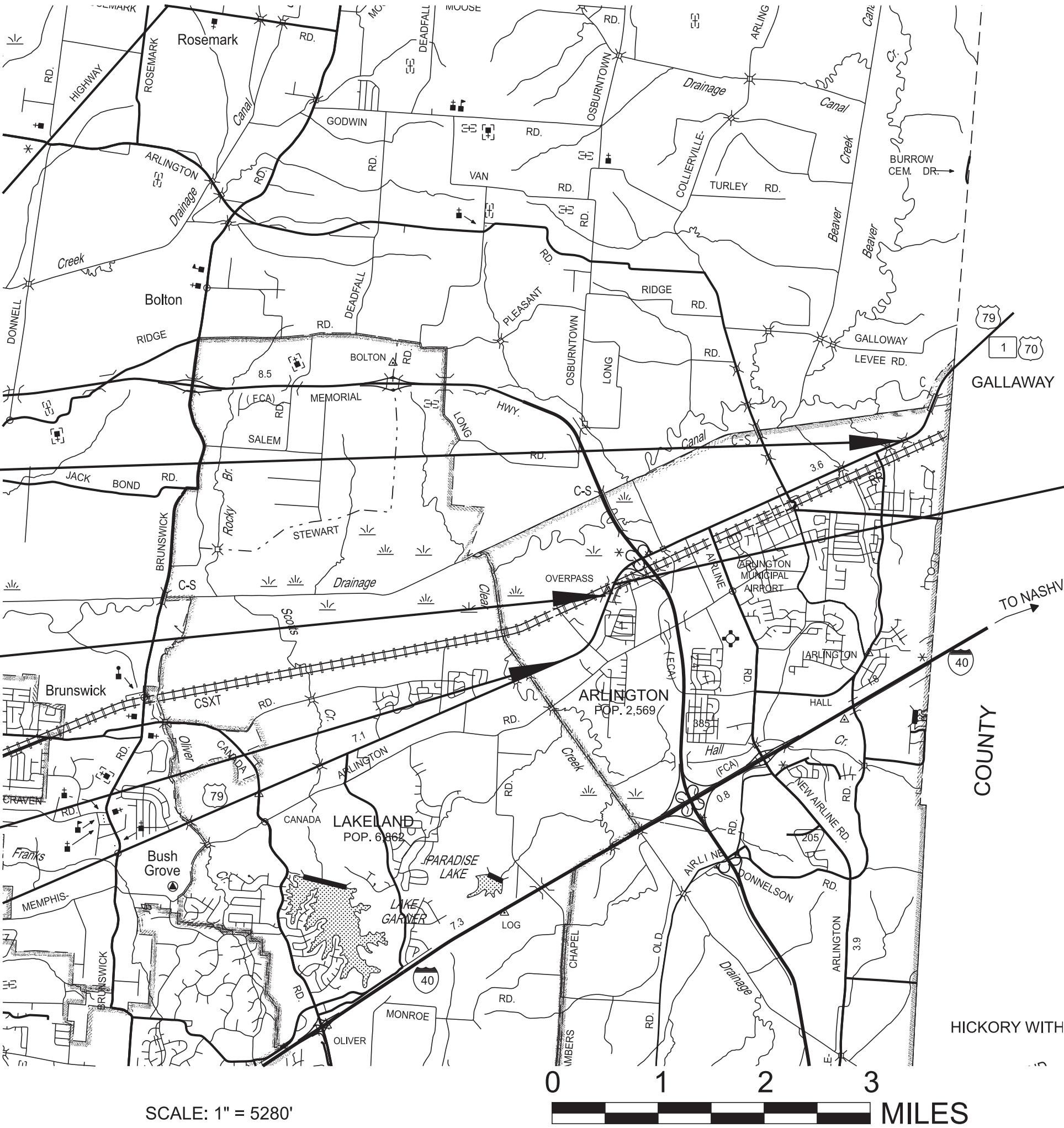
TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4

DESIGNED BY : HDR ENGINEERING, INC.

DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.

P.E. NO. 98043-4283-04

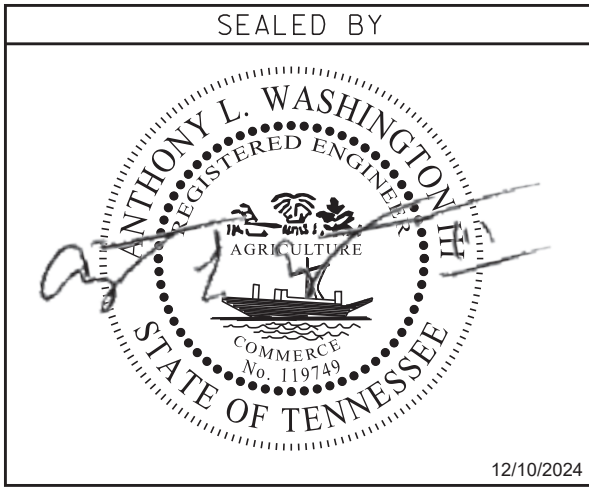
PIN NO. 132518.00



PROJECT LENGTH 4.00 MILES
TOTAL LANE MILES RESURFACED 14.01 MILES

NO EXCLUSIONS

CSXT RAILROAD
#348487B @ S.R. 1 L.M. 26.85
LAT: 35.284942
LONG: -89.691264



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

TRAFFIC COUNTER & WEATHER STATIONS

STATION LOCATION	LOG MILE
TC STATION 42	27.416

TRAFFIC DATA

ADT (2025)	14,077
POSTED SPEED LIMITS	
L.M. 26.00 TO L.M. 27.41	50 MPH
L.M. 27.41 TO L.M. 30.00	40 MPH

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
SIGNATURE SHEET	ROADWAY-SIGN2
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND STANDARD TRAFFIC OPERATIONS DRAWINGS	1A
PRJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUANTITIES	2F
RIGHT-OF-WAY NOTES, UTILITY NOTES, AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	4
BRIDGE PLANS	B-1
NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.	
NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.	

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
DESIGN - TRAFFIC CONTROL		
T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-09-24	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-15A	06-28-19	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES
T-M-16	07-30-24	RUMBLE STRIPE INSTALLATION LAYOUT
T-M-16A	02-03-20	RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-40	03-05-17	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-41	03-05-17	LEFT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-42	03-05-17	CENTER LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-60		FREEWAY RESURFACING SIGNING LAYOUT
T-WZ-63	01-09-24	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64		WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

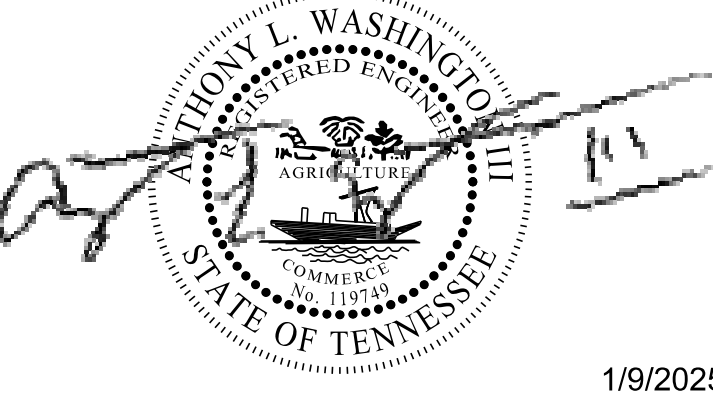
STANDARD TRAFFIC OPERATIONS DRAWINGS

DWG.	REV.	DESCRIPTION
SIGNALS		
T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	1A

REV. 01-09-25: ADDED ROADWAY-SIGN 2 TO INDEX.

SEALED BY



1/9/2025


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,
STANDARD ROADWAY
DRAWINGS, AND
STANDARD TRAFFIC
OPERATIONS DRAWINGS

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION HAZARDOUS MATERIALS	Asbestos Containing Material (ACM) surveys were completed on the following bridges and no asbestos was detected. Please see the reports for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	BRIDGES
EDHZ002	ENVIRONMENTAL DIVISION HAZARDOUS MATERIALS	Bridge No. 79SR0010029 SR-1 EB over CSXRR LM 26.85 (79-SR001-26.85R Bridge No. 79SR0010030 SR-1 WB over CSXRR LM 26.85 (79-SR001-26.85L) Bridge No. 79SR0010047 SR-1 Ramp D over CSXRR LM 27.2 (79-SR001-27.20) Bridge No. 79SR0010049 SR-1 Ramp D over Gulfstream Rd LM 27.23 (79-SR001-27.23) Bridge No. 79SR0010035 SR-1 over Overflow LM 29.81 (79-SR001-29.81)	BRIDGES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	1B

SEALED BY



12/10/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

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(19)
(19)

ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 79S001-F8-008	QUANTITY 79S001-F3-008	TOTAL QUANTITY
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	5313		5313
203-06	WATER	M.G.	28		28
208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	10.5		10.5
303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	3731		3731
307-02.01	ASPHALT CONCRETE MIX (PG70-22) (BPMB-HM) GRADING A	TON	2444		2444
403-01.10	HIGH PERFORMANCE FOG SEALS	S.Y.	1250		1250
403-02.01	TRACKLESS TACK COAT	TON	59		59
411-01.21	LONGITUDINAL JOINT SEALANT	L.M.	12.5		12.5
411-02.10	ACS MIX(PG70-22) GRADING D	TON	11322		11322
411-04	CRACK SEALANT	LB.	500		500
411-12.02	SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	1.6		1.6
411-12.03	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (8IN WIDTH)	L.M.	3.4		3.4
411-12.04	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH)	L.M.	2.7	1.3	4
415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	10524		10524
705-02.10	GUARDRAIL TRANSITION 27IN TO 31IN	EACH		16	16
705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH		11	11
705-06.10	GR TERMINAL TRAILING END (TYPE 13) MASH TL3	EACH		5	5
705-06.20	TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		11	11
706-01	GUARDRAIL REMOVED	L.F.		800	800
712-01	TRAFFIC CONTROL	LS	1		1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	100		100
712-06	SIGNS (CONSTRUCTION)	S.F.	1873		1873
712-08.03	ARROW BOARD (TYPE C)	EACH	2		2
716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH		324	324
716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH		332	332
716-01.23	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH		170	170
716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	760		760
716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	758		758
716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	38	2	40
716-02.07	PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	475		475
716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	1561		1561
716-02.09	PLASTIC PAVEMENT MARKING (LONGITUDINAL CROSS-WALK)	L.F.	506		506
716-02.12	PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	0.5		0.5
716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	2		2
716-03.06	PLASTIC WORD PAVEMENT MARKING (SIGNAL AHEAD)	EACH	4		4
716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	17		17
716-08.04	REMOVAL OF PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	626		626
716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.		17.4	17.4
717-01	MOBILIZATION	LS	1		1
730-14.02	SAW SLOT	L.F.	2974		2974
730-14.03	LOOP WIRE	L.F.	6748		

FOOTNOTES

- (1) TO BE USED AS DIRECTED BY THE ENGINEER. NO DIRT OR DEBRIS TO BE LEFT ON SHOULDER, THIS WORK TO BE PERFORMED BEFORE ALL OTHER OPERATIONS.
- (2) INCLUDES 2,397 TONS FOR BREAKOUTS.
- (3) TO BE USED FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01
- (4) TO BE USED AS DIRECTED BY THE TDOT ENGINEER IN BETWEEN L.M. 26.95 - L.M. 27.30 ON THE GORE AREAS AND OUTSIDE SHOULDERS.
- (5) INCLUDES 5 TONS FOR DRIVEWAYS, CITY STREETS, FIELD ENTRANCES, AND BUSINESS ENTRANCES.
- (6) TO BE USED FOR SEALING OF ALL SURFACE LAYER CONSTRUCTION JOINTS ALONG THE TRAVEL LANES AND SHOULDERS AS DIRECTED BY THE ENGINEER. USE CRAFTCO PAVEMENT JOINT ADHESIVE #34524. PAVON JOINT ADHESIVE BY PAVON CORPORATION OR DENSO TAPE BY DENSO
- (7) INCLUDES 676 TONS FOR DRIVEWAYS, CITY STREETS, FIELD ENTRANCES, AND BUSINESS ENTRANCES. INCLUDES 345 TONS FOR SPOT LEVELING.
- (8) TO BE USED ON LEFT OUTSIDE SHOULDER L.M. 26.95 TO L.M. 27.30, L.M. 37.35 TO L.M. 27.55, L.M. 27.84 TO 28.09, L.M. 28.76 TO L.M. 28.80 AND 29.62 TO 29.77; RIGHT OUTSIDE SHOULDER L.M. 27.03 TO L.M. 27.20, L.M. 27.30 TO L.M. 27.84, L.M. 28.76 TO L.M. 28.80 AND L.M. 29.62 TO L.M. 29.77
- (9) TO BE USED ON LEFT OUTSIDE SHOULDER L.M. 26.18 TO L.M. 26.95, L.M. 27.30 TO L.M. 27.35, L.M. 27.55 TO L.M. 27.84, L.M. 28.09 TO 28.33 AND L.M. 29.03 TO 29.62; RIGHT OUTSIDE SHOULDER L.M. 26.18 TO L.M. 27.03, L.M. 27.20 TO L.M. 27.30, L.M. 27.84 TO L.M. 27.95, L.M. 28.80 TO L.M. 28.90 AND L.M. 29.03 TO 29.62 AND LEFT INSIDE SHOULDER L.M. 27.03 TO 27.20
- (10) TO BE USED ON LEFT OUTSIDE SHOULDER L.M. 26.00 TO 26.18, L.M. 28.33 TO 28.76, L.M. 28.80 TO L.M. 29.03 AND L.M. 29.77 TO 30.00; RIGHT OUTSIDE SHOULDER L.M. 26.00 TO 26.18, L.M. 28.33 TO L.M. 28.76, L.M. 28.90 TO L.M. 29.03 AND L.M. 29.77 TO L.M. 30.00; LEFT INSIDE SHOULDER L.M. 26.32 TO L.M. 27.03 AND L.M. 27.20 TO L.M. 27.55; AND RIGHT INSIDE SHOULDER L.M. 26.95 TO L.M. 27.55
- (11) INCLUDES 670 TONS FOR DRIVEWAYS, CITY STREETS, FIELD ENTRANCES, AND BUSINESS ENTRANCES.
- (12) SEE GUARDRAIL TABULATION ON SHEET 2F.
- (13) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (14) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION)
- (15) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
- (16) ITEM TO BE USED AS PERMANENT MARKING ONLY
- (17) ALL STOP BARS ON SIDE STREETS WILL BE RETRACED AS DIRECTED BY THE ENGINEER.
- (18) ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.
- (19) SEE LOOP REPLACEMENT TABULATION ON SHEET 2F
- (20) TO BE USED AS DIRECTED BY TDOT ENGINEER IN BETWEEN L.M. 26.95 - L.M 27.30.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	2

REV. 01-09-25: ADDED ITEM 716-08.04 AND FOOTNOTE (20), REVISED FOOTNOTE (4).

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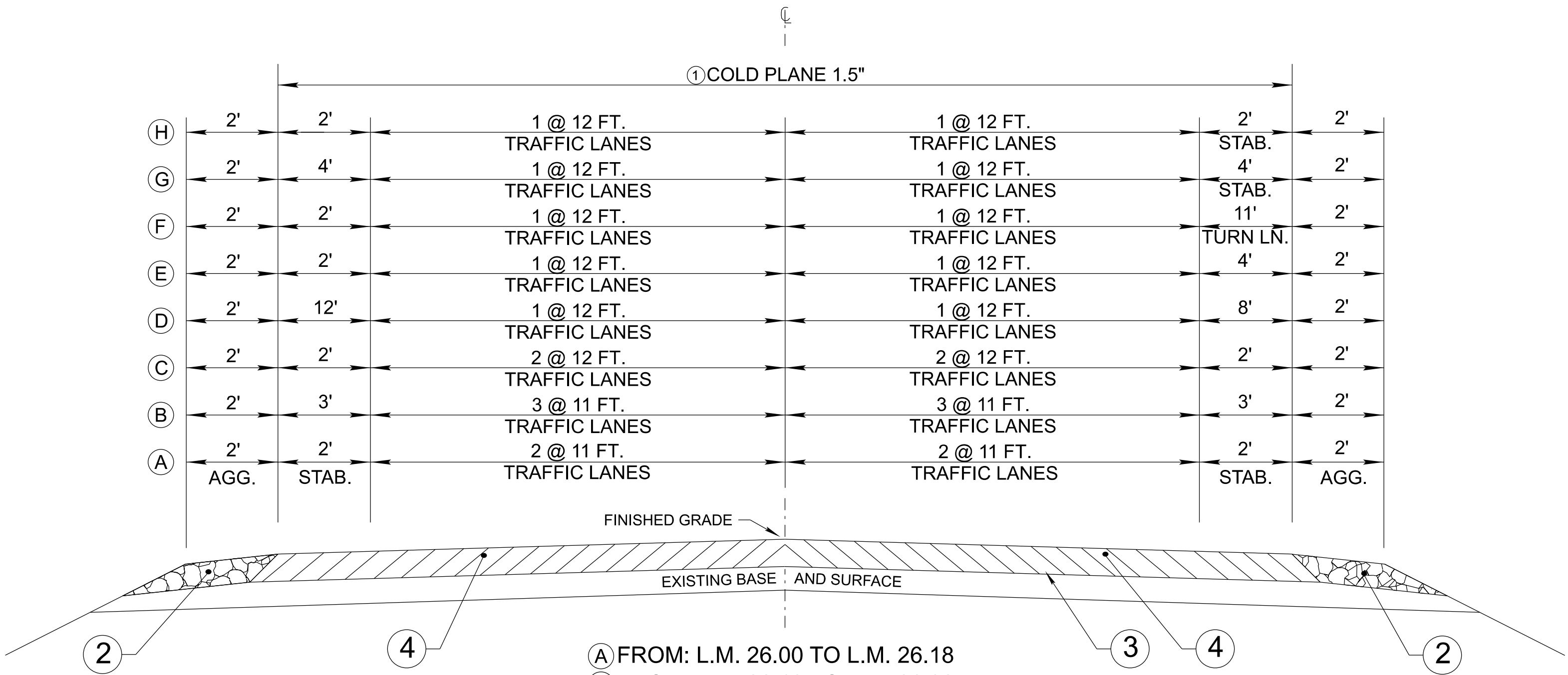
1/9/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

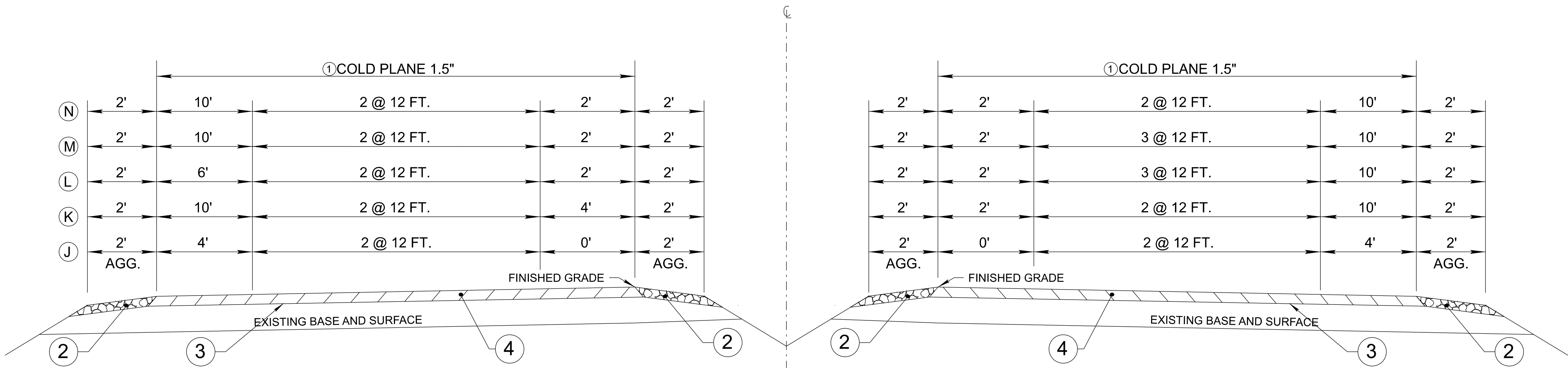
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	2B

REV. 01-09-25: REMOVED PORTION OF PAVEMENT
HATCHING FROM TYPICAL SECTION L.M. 26.95 TO
L.M 27.03 ON RT.

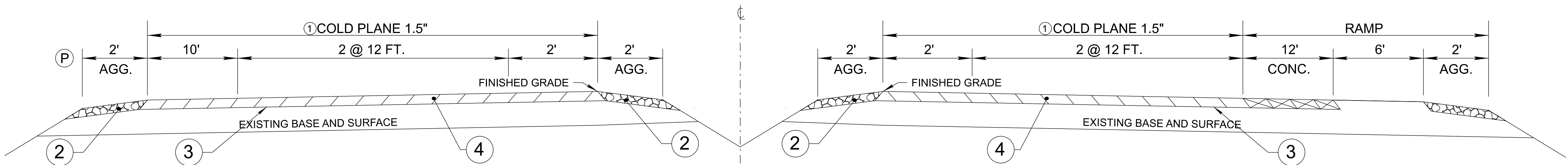


- ② FROM: L.M. 26.00 TO L.M. 26.18
- ③ FROM: L.M. 26.18 TO L.M. 26.32
- ④ FROM: L.M. 28.33 TO L.M. 28.76
- ⑤ FROM: L.M. 28.76 TO L.M. 28.80
- ⑥ FROM: L.M. 28.80 TO L.M. 28.90
- ⑦ FROM: L.M. 28.90 TO L.M. 29.03
- ⑧ FROM: L.M. 29.03 TO L.M. 29.62
- ⑨ FROM: L.M. 29.77 TO L.M. 30.00

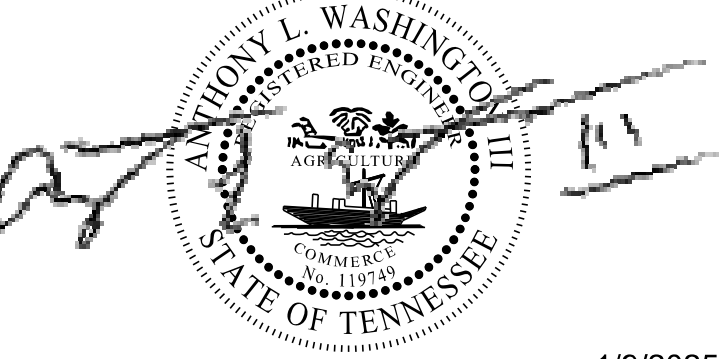
SEE SHEET 2B1 FOR PAVEMENT SCHEDULE



- ⑩ FROM: L.M. 26.32 TO L.M. 26.95
- ⑪ FROM: L.M. 27.03 TO L.M. 27.20
- ⑫ FROM: L.M. 27.30 TO L.M. 27.35
- ⑬ FROM: L.M. 27.35 TO L.M. 27.40
- ⑭ FROM: L.M. 27.40 TO L.M. 27.55



- ⑮ FROM: L.M. 26.95 TO L.M. 27.03

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1/9/2025

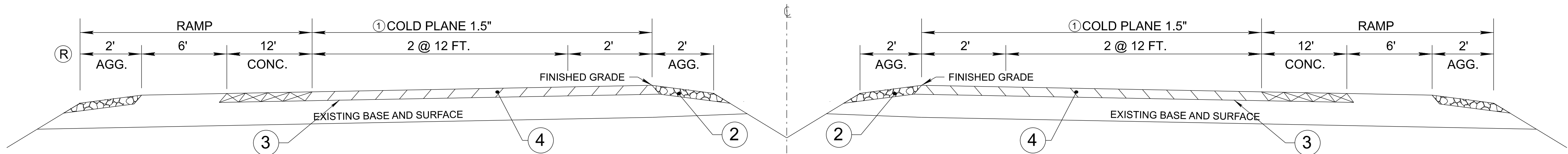
NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

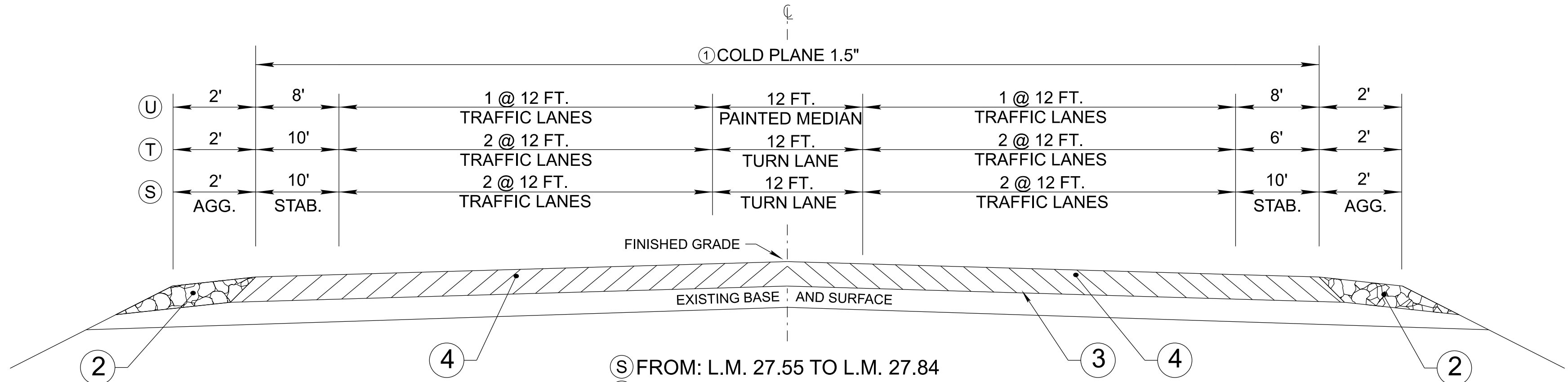
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	2B1

REV. 01-09-25: REMOVED PORTION OF PAVEMENT
HATCHING FROM TYPICAL SECTION L.M. 27.20 TO
L.M. 27.30 ON RT & LT.



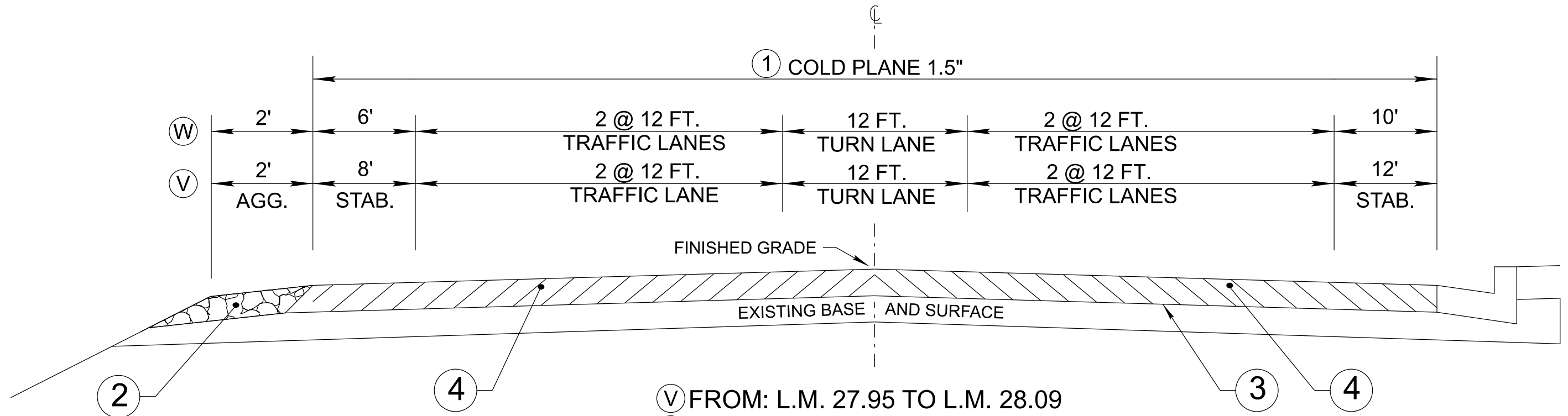
FROM: L.M. 27.20 TO L.M. 27.30



FROM: L.M. 27.55 TO L.M. 27.84

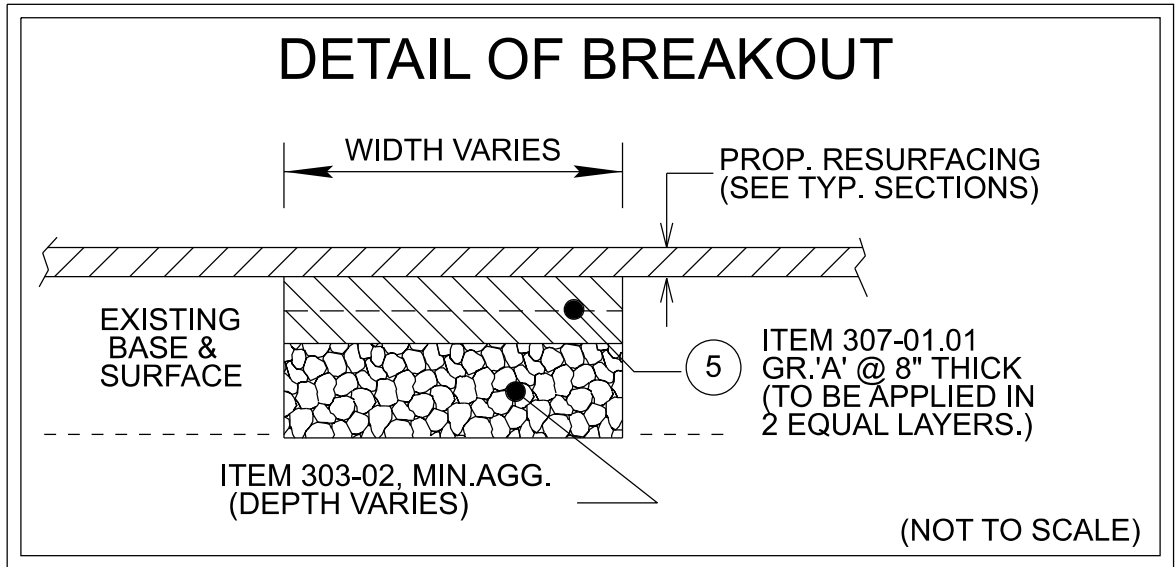
FROM: L.M. 27.84 TO L.M. 27.95

FROM: L.M. 29.62 TO L.M. 29.77



FROM: L.M. 27.95 TO L.M. 28.09

FROM: L.M. 28.09 TO L.M. 28.33



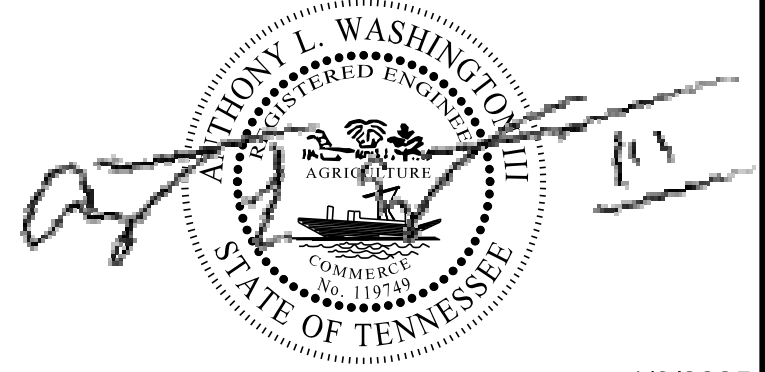
PROPOSED PAVEMENT SCHEDULE

1	COLD PLANING @ 1.50"± THICK (APPROX. 157.5 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TON)
2	MINERAL AGGREGATE @ 2" THICK ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"
3	TRACKLESS TACK COAT ITEM 403-02.01 TRACKLESS TACK COAT SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
4	ASPHALTIC CONCRETE SURFACE (HOT MIX) @ 1.50"± THICK (APPROX. 159.0 LBS./S.Y.) ITEM 411-02.10 ACS MIX (PG70-22) GRADING "D"
5	BITUMINOUS COURSE (BLACK BASE) @ 6.00"± THICK (APPROX. 690.0 LBS./S.Y.) ITEM 307-02.01 ASPHALT CONCRETE MIX (PG70-22)(BPMB-HM) GRADING "A" (THIS ITEM TO BE USED FOR BREAKOUT ONLY)

BRIDGE NOTES

BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	VERTICAL CLEARANCE		BRIDGE NOTES
				EBL	WBL	
79SR0010029	26.850R	CSXT	159' - 3 1/2"			REPLACE BRIDGE JOINTS AND PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (SEE BRIDGE PLANS)
79SR0010030	26.850L	CSXT	165' - 0"			REPLACE BRIDGE JOINTS AND PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (SEE BRIDGE PLANS)
79SR0010047	27.200	CSXT	186' - 3"			PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (SEE BRIDGE PLANS)
79SR0010049	27.230	GULFSTREAM AVE.	177' - 0"			PROVIDE DECK REPAIR ITEM - SEAL WITH TYPE 1 THIN EPOXY OVERLAY (SEE BRIDGE PLANS)
79SR0010046	27.240	SR-385		17' - 4"	16' - 3"	TO MAINTAIN VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
79SR0010045	27.260	SR-385		18' - 6"	18' - 0"	TO MAINTAIN VERTICAL CLEARANCE OF THE UNDERPASS COLD PLANE TO MATCH THE THICKNESS OF TREATMENT
79SR0010031	27.470R	BRANCH	32' - 6"			PAVE WITH PLANS MIX/TREATMENT TYPE
79SR0010031	27.470L	BRANCH	32' - 6"			PAVE WITH PLANS MIX/TREATMENT TYPE
79SR0010033	29.320	BRANCH	25' - 6"			PAVE WITH PLANS MIX/TREATMENT TYPE
79SR0010035	29.810	OVERFLOW	24' - 6"			SEAL WITH TYPE 1 THIN EPOXY OVERLAY (SEE BRIDGE PLANS)
79SR0010037	29.970	OVERFLOW	228' - 0"			LEAVE AS IS

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NOT TO SCALE

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6" LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. SHOULDERS SHALL BE BROOMED AND DE-GRASSED AND MATERIAL SHALL BE PICKED UP AND REMOVED. THIS WILL BE PAID FOR UNDER ITEM NO. 208-01.05.

b. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS
- (20) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.
- PAVEMENT
- PAVING
- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- RESURFACING
- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

(6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.

(7) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.

(8) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.

(9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.
- SIGNING
- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.
- SIGNALIZATION
- (11) THE PROJECT ENGINEER SHALL NOTIFY THE LOCAL GOVERNMENTAL AGENCY RESPONSIBLE FOR TRAFFIC CONTROL MAINTENANCE AT LEAST ONE DAY IN ADVANCE OF THE COLD PLANING ACTIVITY AT SIGNALIZED INTERSECTIONS WHERE DETECTOR LOOPS ARE ON THE PAVEMENT. THE MAINTAINING AGENCY WILL THEN BE RESPONSIBLE FOR DISCONNECTING THE LOOP DETECTORS AND MAKING ANY NECESSARY TIMING ADJUSTMENTS IN THE SIGNAL CONTROLLER PRIOR TO THE CONSTRUCTION.

(12) THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR SUPPLYING THE CONTRACTOR WITH AS BUILT SIGNAL PLANS AT THE PRE-CONSTRUCTION CONFERENCE. THESE PLANS WILL PROVIDE THE CONTRACTOR WITH THE DESIRED LOCATION FOR DETECTOR LOOP REPLACEMENT.

(14) LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 730 OF THE STANDARD SPECIFICATIONS.
- CONSTRUCTION WORK ZONE & TRAFFIC CONTROL
- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

(2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

(3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.


(4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

(5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

(9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.
- | | | | |
|--------|------|----------------|-----------|
| TYPE | YEAR | PROJECT NO. | SHEET NO. |
| RESURF | 2025 | NH/HSIP-1(495) | 2C |
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- SEALED BY



12/10/2024
- STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES
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SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (2) WHEN A PERFORMANCE GRADE (PG) ASPHALT MIX WITH PROPERTIES GREATER THAN THAT OF PG64-22 IS CALLED FOR ON RESURFACING PLANS AND IS THE ONLY ASPHALT GRADE ON THE PROJECT, THE CONTRACTOR HAS THE OPTION OF USING EITHER THE ASPHALT GRADE SHOWN IN THE PLANS OR AN ASPHALT GRADE EQUAL TO OR BETTER THAN PG64-22 FOR DRIVEWAYS AND BUSINESS ENTRANCES, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE MATERIAL WILL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THE ASPHALT OR ASPHALT MIX. THE MATERIAL TICKETS SHALL BE MARKED "FOR DRIVEWAYS AND BUSINESS ENTRANCES ONLY" AT THE POINT OF MANUFACTURE.
- (3) THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (4) THE OUTSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.
- (4) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

JOINT SEALANTS

- (1) THE CONTACT SURFACE OF TRANSVERSE JOINTS AND LONGTUDINAL JOINTS IN THE SURFACE LIFT SHALL BE SEALED BY APPLYING JOINT SEALANT PRIOR TO THE PLACEMENT OF ADDITIONAL ASPHALT AGAINST THE PREVIOUSLY PLACED MATERIAL. MANUFACTURERE'S RECOMMENDATIONS SHALL BE FOLLOWED IF THE MATERAIL NEEDS TO BE RE-HEATED, AND WHEN PLACING THE THIN, UNIFORM COAT.
- (3) PRIOR TO APPLICATION OF THE SEALANT, THE FACE OF THE JOINT SHALL BE THOROUGHLY DRY AND FREE FROM DUST OR ANY OTHER MATERAIL THAT WOULD PREVENT PROPER SEALING. ALL JOINTS SHALL BE SWEPT OR BLOWN FREE OF LOOSE MATERAIL, DIRT, VEGETATION, AND OTHER DEBRIS BY MEANS OF COMPLRESSED AIR OR A POWER SWEEPER.

TRUCK AND VEHICLE TRAFFIC SHALL NOT DRIVE ACROSS A SEALED JOINT UNTIL IT HAS DRIED SUFFICIENT TO PREVENT DAMAGE FROM TRACKING.

RAILROAD NOTES

- (2) THE CONTRACTOR SHALL COLD PLANE AND RESURFACE THE ROADWAY NEAR AND UNDER THE RAILROAD'S UNDERPASS/BRIDGE STRUCTURE SO AS NOT TO DIMINISH THE EXISTING VERTICAL CLEARANCE BETWEEN THE HIGHEST POINT OF THE ROADWAY PAVEMENT AND THE LOWEST POINT OF THE RAILROAD BRIDGE SUPERSTRUCTURE.
- (3) THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE CSX TRANSPORTATION TRACK FACILITIES AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY CSX TRANSPORTATION SO TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAIN TRAFFIC IN ANY MANNER.
- (4) THE CONTRACTOR SHALL NOT, FOR ANY REASON, STORE ANY OF HIS CONSTRUCTION EQUIPMENT OR DUMP WASTE MATERIALS ON THE RAILROAD'S RIGHT-OF-WAY.
- (5) THE CONTRACTOR IS PROHIBITED FROM INSTALLING ANY CONSTRUCTION SIGNS, MESSAGE BOARDS, ARROW BOARDS AND/OR OTHER TEMPORARY TRAFFIC CONTROL SIGNS OR DEVICES WITHIN THE RAILROAD'S RIGHT-OF-WAY. IN ALL CASES, THESE SIGNS AND DEVICES MUST NEVER BE PLACED IN A POSITION SO AS TO OBSTRUCT THE VIEW OF ANY ADVANCE WARNING DEVICES SUCH AS CROSSBUCK SIGNS, CROSSING SIGNALS, ETC.
- (6) THE CONTRACTOR SHALL NOTIFY AND COORDINATE HIS WORK AT THE RAILROAD CROSSING WITH THE FOLLOWING REPRESENTATIVE OF THE RAILROAD:

CSX

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE

CROUCH ENGINEERING, INC.

MR. SCOTT VICK, P.E. PROJECT DIRECTOR

CSX TRANSPORTATION INC.

5115 MARYLAND WAY, SUITE 225

BRENTWOOD, TN 37027

PHONE: (615) 791-0630

E-MAIL: SVICK@CROUCHENGINEERING.COM

- (7) ALL WORK ON, OVER, UNDER OR ADJACENT TO CSXT RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH CSXT SPECIAL PROVISIONS. SEE AGREEMENT/STATE CONTRACT SPECIAL PROVISIONS 105C FOR CSXT REQUIREMENTS

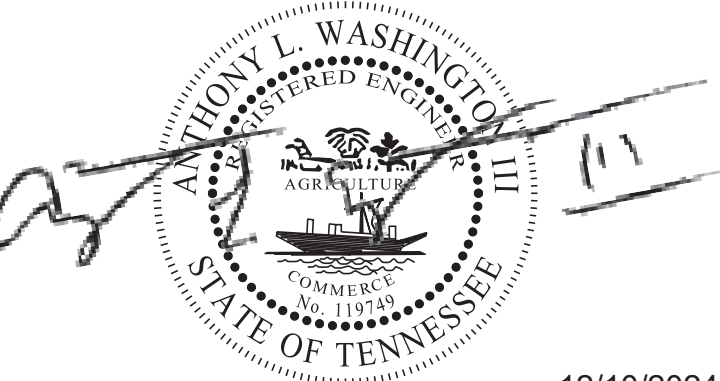
CSXT SAFETY NOTES

- (1) COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (2) ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE ALLOWED ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (3) ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (4) CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY
- (5) ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (6) ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (7) AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.

- (8) THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPOMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (9) THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT'S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS
- (10) PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD'S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (11) ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (12) NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (13) ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORKDAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (14) ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (15) "ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD'S REPRESENTATIVE FIVE (5) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
- (16) ALL SOILS EXCAVATED WITHIN CSXT'S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT'S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	2D

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12/10/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4)THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9)THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11)SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15)IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

PROJECT COMMITMENTS

- (5)SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (6)THIS PROJECT INCLUDES COLD PLANING EXISTING ASPHALT, RESURFACING, PAVEMENT MARKINGS, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1)IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

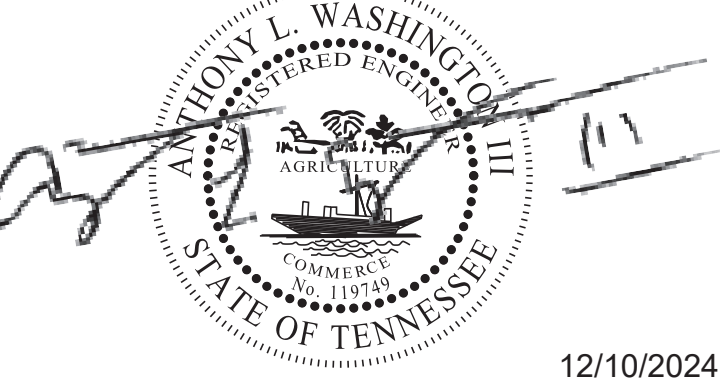
- (6)EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8)THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29)THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30)THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31)CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32)WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33)IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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ENVIRONMENTAL
NOTES


ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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ENVIRONMENTAL
NOTES


TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x	W		
E5-1	EXIT	72"	x	60"	30	30
E5-2	EXIT OPEN	48"	x	36"	12	12
G20-1	ROAD WORK NEXT 4 MILES	48"	x	24"	8	16
G20-2	END ROAD WORK	48"	x	24"	8	18
R1-2	YIELD	36"X36"X36"			9	9
R3-8M	ADVANCE INTERSECTION LANE CONTROL	30"	x	30"	6	6
W3-2	YIELD AHEAD	48"	x	48"	16	16
W4-1R	MERGING TRAFFIC	48"	x	48"	16	16
W4-2L	LEFT LANE ENDS SYMBOL	48"	x	48"	16	16
W4-2R	RIGHT LANE ENDS SYMBOL	48"	x	48"	16	16
W8-11	UNEVEN LANES	48"	x	48"	16	24
W8-15	GROOVED PAVEMENT	48"	x	48"	16	24
W8-15P	MOTORCYCLE PLAQUE	30"	x	24"	5	24
W12-1	DOUBLE ARROW	48"	x	48"	16	1
W20-1	ROAD WORK 1 MILE	48"	x	48"	16	1
W20-1	ROAD WORK 1/2 MILE	48"	x	48"	16	2
W20-1	ROAD WORK 1000 FT	48"	x	48"	16	2
W20-1	ROAD WORK 500 FT	48"	x	48"	16	1
W20-1	ROAD WORK AHEAD	48"	x	48"	16	16
W20-5	CENTER LANE CLOSED 1000 FT	48"	x	48"	16	1
W20-5	CENTER LANE CLOSED 500 FT	48"	x	48"	16	1
W20-5L	LEFT LANE CLOSED 1/2 MILE	48"	x	48"	16	2
W20-5L	LEFT LANE CLOSED 1500 FT	48"	x	48"	16	2
W20-5L	LEFT LANE CLOSED 1000 FT	48"	x	48"	16	1
W20-5R	RIGHT LANE CLOSED 1/2 MILE	48"	x	48"	16	2
W20-5R	RIGHT LANE CLOSED 1500 FT	48"	x	48"	16	2
W20-5R	RIGHT LANE CLOSED 1000 FT	48"	x	48"	16	1
W20-7	FLAGGER SYMBOL	48"	x	48"	16	2
W20-7A	FLAGGER AHEAD	48"	x	48"	16	3
W21-5	SHOULDER WORK	48"	x	48"	16	4
THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES.					TOTAL	1873

PROPOSED GUARDRAIL (RESURFACING)							
SIDE		LOG MILE	GUARDRAIL			TERMINAL ANCHORS	
			GUARDRAIL BEAM TRANSITION 27 IN TO 31 IN 705-02.10 (EACH)	EARTH PAD FOR TYPE 38 GR END TREATMENT 705-04.09 (EACH)	GUARDRAIL REMOVED 706-01 (L.F.)	TYPE 13 MASH TL3 (9.375') 705-06.10 (EACH)	TYPE 38 MASH TL3 (26.896') 705-06.20 (EACH)
LT	RT						
X		26.710	1		50	1	
	X	26.815	1	1	50		1
	X	26.936	1		50	1	
	X	27.115	1		50	1	
	X	27.219	1	1	50		1
X		27.232	1		50	1	
	X	27.256	1		50	1	
X		27.810	1	1	50		1
	X	27.815	1	1	50		1
X		29.300	1	1	50		1
	X	29.302	1	1	50		1
X		29.333	1	1	50		1
	X	29.336	1	1	50		1
X		29.955	1	1	50		1
	X	29.956	1	1	50		1
X		30.027	1	1	50		1
TOTALS			16	11	800	5	11

SIGNALIZATION TABULATION (RESURFACING)		
INTERSECTION	SAW SLOT (L.F.)	LOOP WIRE (L.F.)
	730-14.02	730-14.03
SR-1 @ LAMB RD. (L.M. 26.261)	1370	3140
SR-1 @ AIRLINE RD. (L.M. 27.950)	919	2038
SR-1 @ CHESTER ST. (L.M. 28.760)	351	702
SR-1 @ MILTON WILSON RD. (L.M. 29.680)	334	868
TOTAL	2974	6748

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TABULATED
QUANTITIES

RIGHT-OF-WAY NOTES

RIGHT-OF-WAY

- (12) EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

UTILITY NOTES

UTILITY

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

SEWER:


ARLINGTON PUBLIC WORKS
P.O. BOX 507
ARLINGTON, TN 38002
CONTACT: TERRY PERKINS
OFFICE: 901-867-4980
EMAIL: TPERKINS@TOWNOFARLINGTON.ORG

GAS, WATER, ELECTRIC:

MLG&W
220 SOUTH MAIN ST.
MEMPHIS, TN 38103
CONTACT: ANGEL DABNEY
OFFICE: 901-288-3419
EMAIL: ADABNEY@MLGW.ORG

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DEPARTMENT OF TRANSPORTATION

RIGHT-OF-WAY
NOTES,
UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.
- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.
- FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.
- SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.
- IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
- B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:
1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.


2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH/HSIP-1(495)	4

SEALED BY



12/10/2024

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

11/14/2024 9:08:26 PM C:\PWORKING\EA\ST01\ID343533\02D.DGN



2024.11.19 08:44:00 -06'00'

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

SHEET NAME

SHEET NO.

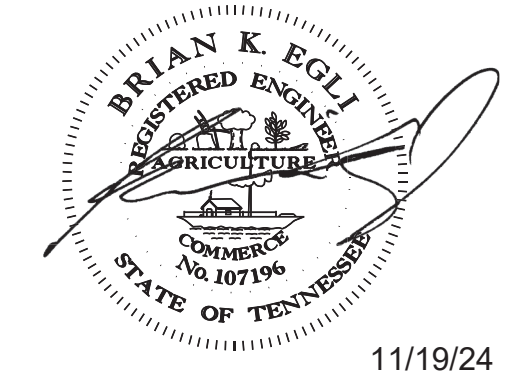
SIGNATURE SHEET _____ STRUCTURE-SIGN 1
BRIDGE PLANS _____ B1 THRU B15

SIGNATURE SHEET

PIN 132518.00

[illegible]

<u>INDEX OF REFERENCE DRAWINGS</u>	<u>DWG. NO.</u>
LAYOUT OF BRIDGE	F-14-48
BRIDGE REPAIR DETAILS	BR-69-59
LAYOUT OF BRIDGE WIDENING	F-15-121
BRIDGE REPAIR DETAILS	BR-69-46
BRIDGE REPAIR DETAILS	BR-69-48
LAYOUT	M-288-160
SUPERSTRUCTURE	M-288-162
LAYOUT	M-288-173
SUPERSTRUCTURE	M-288-176
LAYOUT OF BRIDGE	D-5-43
TYPICAL SECTION	BR-96-5



STATE OF TENNESSEE

INDEX OF DRAWINGS
79-SR1-26.85R & L
79-SR1-27.20
OVER
CSXT RR
79-SR1-27.23
OVER
GULFSTREAM AVE.
79-SR1-29.81
OVER
OVERFLOW

B1

PIN 132518.00

PROJECT NO.		YEAR	SHEET NO.
79S001-M3-011		2025	B2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES					
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIR L.F.	604-10.53 CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
79-SR1-26.85R OVER CSXT RR (79SR0010029)	F-14-48 BR-69-59	EXPANSION JOINT REPAIR TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	200		558
79-SR1-26.85L OVER CSXT RR (79SR0010030)	F-15-121 BR-69-46 BR-69-48	EXPANSION JOINT REPAIR TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	206		605
79-SR1-27.20 OVER CSXT RR (79SR0010047)	M-288-160 M-288-162	CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		8	813
79-SR1-27.23 OVER GULFSTREAM AVE. (79SR0010049)	M-288-173 M-288-176	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)			774
79-SR1-29.81 OVER OVERFLOW (79SR0010035)	D-5-43 BR-96-5	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)			86
TOTAL			406	8	2836

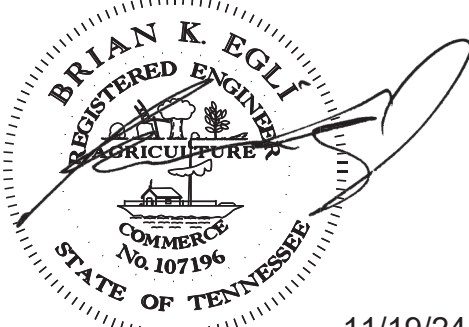
EXPANSION JOINT REPAIR NOTES

THE JOINT HEADER SYSTEM SHALL BE FROM OPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM OPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM OPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM OPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE OPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION,
ESTIMATED QUANTITIES, AND
EXPANSION JOINT REPAIR
NOTES
79-SR1-26.85R & L
79-SR1-27.20
OVER
CSXT RR
79-SR1-27.23
OVER
GULFSTREAM AVE.
79-SR1-29.81
OVER
OVERFLOW
BR. NOS. 79SR0010029
79SR0010030
79SR0010047
79SR0010049
79SR0010035
SHELBY COUNTY
2025

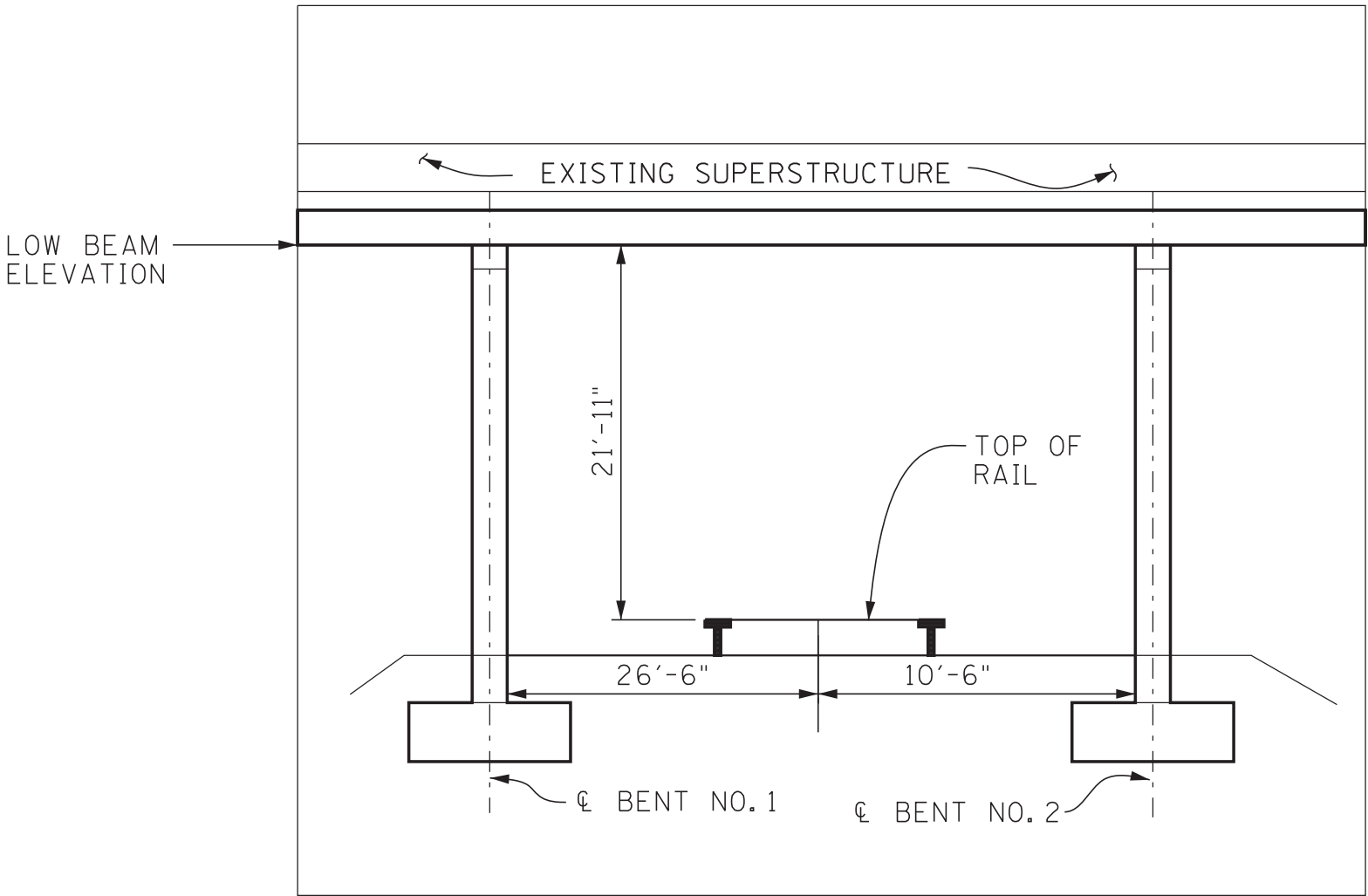
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DRAWN BY Z.HAYNES DATE 7/24
SUPERVISED BY K. MARTINKO DATE 7/24
CHECKED BY _____ DATE _____

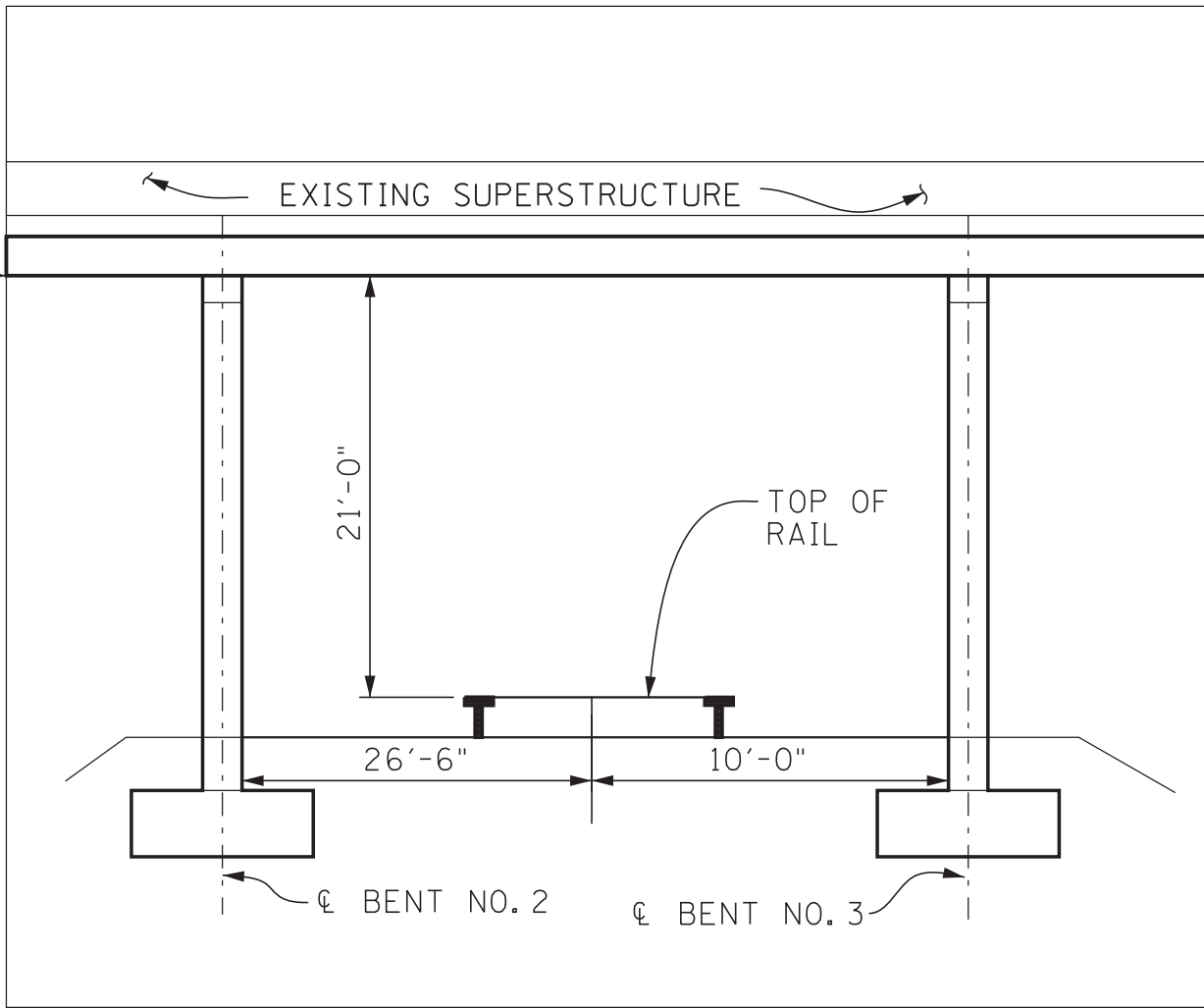
B3

PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B5

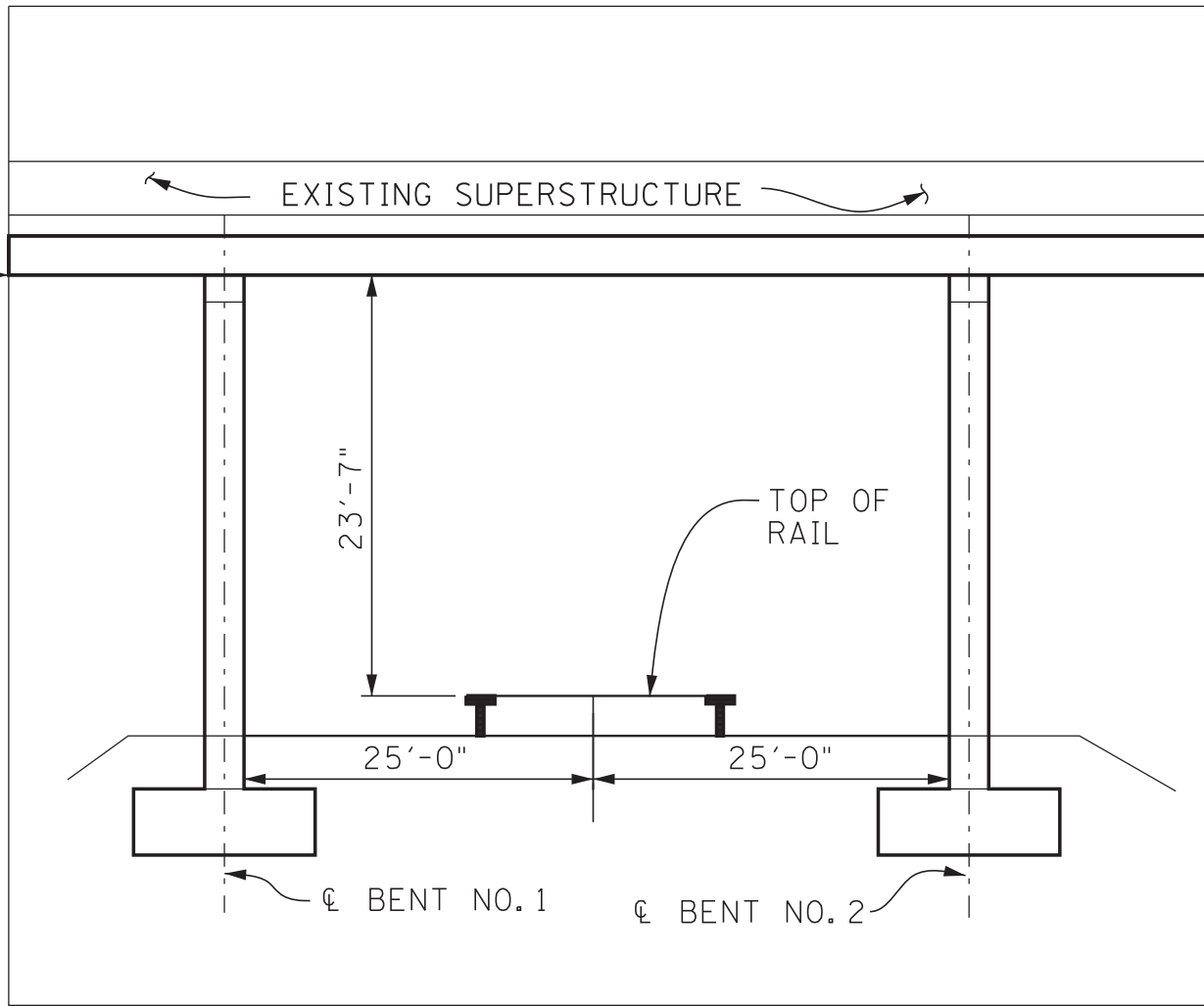
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



CSX RAILROAD
EXISTING CLEARANCE
FOR 79SR0010029



CSX RAILROAD
EXISTING CLEARANCES
FOR 79SR0010030



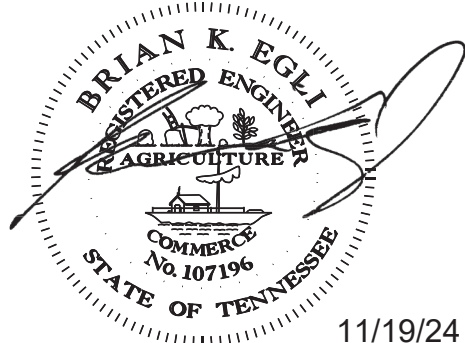
CSX RAILROAD
EXISTING CLEARANCES
FOR 79SR0010047

SPECIAL RAILROAD NOTES- BRIDGE REPAIR

- (1) TEMPORARY CONSTRUCTION CLEARANCES SHALL BE SUBJECT TO APPROVAL BY CSXT. TYPICALLY, REDUCTION IN CLEARANCE FOR CONSTRUCTION IS NOT PERMITTED.
- (2) CONTRACTOR SHALL MAINTAIN ALL DITCHES AND DRAINAGE STRUCTURES FREE OF SILT OR OTHER OBSTRUCTIONS THAT MAY RESULT FROM THEIR OPERATIONS. CONTRACTOR, UPON COMPLETION OF THE PROJECT, SHALL LEAVE CSXT PROPERTY IN A NEAT CONDITION, SATISFACTORY TO THE CSXT REPRESENTATIVE.
- (3) THE CONTRACTOR SHALL PROVIDE, INSTALL AND MAINTAIN A GEOTEXTILE FABRIC BALLAST PROTECTION SYSTEM TO PREVENT DEBRIS AND FINES FROM FOULING THE BALLAST. THE BALLAST PROTECTION SYSTEM SHALL EXTEND 25' BEYOND THE OUTER LIMITS OF THE BRIDGE ON BOTH SIDES.
- (4) ALL WORK ON, OVER, UNDER, OR ADJACENT TO CSXT RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH CSXT SPECIAL PROVISIONS LOCATED WITHIN THE CSXT PUBLIC PROJECT INFORMATION MANUAL AT THE FOLLOWING LINK:
[HTTPS://WWW.CSX.COM/INDEX.CFM?LIBRARY/FILES/ABOUT-US/PROPERTY/PUBLIC-PROJECT-MANUAL/](https://www.csx.com/index.cfm?library/files/about-us/property/public-project-manual/)
- (5) ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE DONE ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (6) ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (7) CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY (WILL NOT BE USED FOR CAT PLOWING).
- (8) ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (9) ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (10) ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (11) AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (12) SEE AGREEMENTS / STATE CONTRACT SPECIAL PROVISION 105C FOR ADDITIONAL INFORMATION REGARDING WORK UP TO AND NEAR THE RAILROAD AT-GRADE CROSSING, REGARDING FLAGGING AND INSURANCE REQUIREMENTS
- (13) THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (14) THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT'S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS
- (15) PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD'S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (16) THE CONTRACTOR SHALL REPLACE ALL RR ADVANCE WARNING SIGNS, RXR PAVEMENT MARKINGS AND STOP LINES AT APPROACHES TO THIS GRADE CROSSINGS, IN ACCORDANCE WITH MUTCD GUIDELINES.
- (17) THE FINISHED ROADWAY SURFACE SHALL BE AT THE SAME ELEVATION AS THE RAILROAD CROSSING SURFACE, FOR A DISTANCE OF 30' ON EACH SIDE OF TRACK(S).
- (18) ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (19) THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:

CSXT GENERAL ENGINEERING CONSULTANT DESIGNATE
CROUCH ENGINEERING, INC.
5115 MARYLAND WAY, SUITE 225
BRENTWOOD, TN 37027
ATTN: MR. SCOTT VICK, P.E. PROJECT DIRECTOR
PHONE: (615) 791-0630
EMAIL: SVICK@CROUCHENGINEERING.COM

DESIGNED BY _____ DATE _____
DRAWN BY _____ DATE 7/24
SUPERVISED BY _____ DATE 7/24
CHECKED BY _____ DATE _____

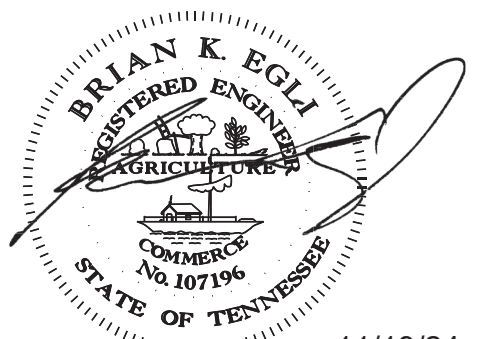


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RAILROAD NOTES
79-SR1-26.85R & L
79-SR1-27.20
OVER
CSXT RR
79-SR1-27.23
OVER
GULFSTREAM AVE.
79-SR1-29.81
OVER
OVERFLOW
BR. NOS. 79SR0010029
79SR0010030
79SR0010047
79SR0010049
79SR0010035
SHELBY COUNTY
2025

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DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____



PLAN VIEW
79-SR1-26.85R
OVER
CSXT RR
BR. NO. 79SR0010029
SHELBY COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B7

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PHASE I CONSTRUCTION

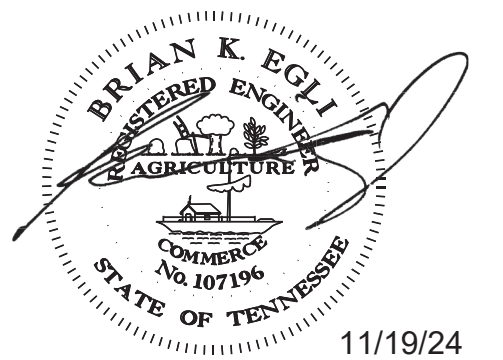
EASTBOUND BRIDGE

PHASE II CONSTRUCTION

B7

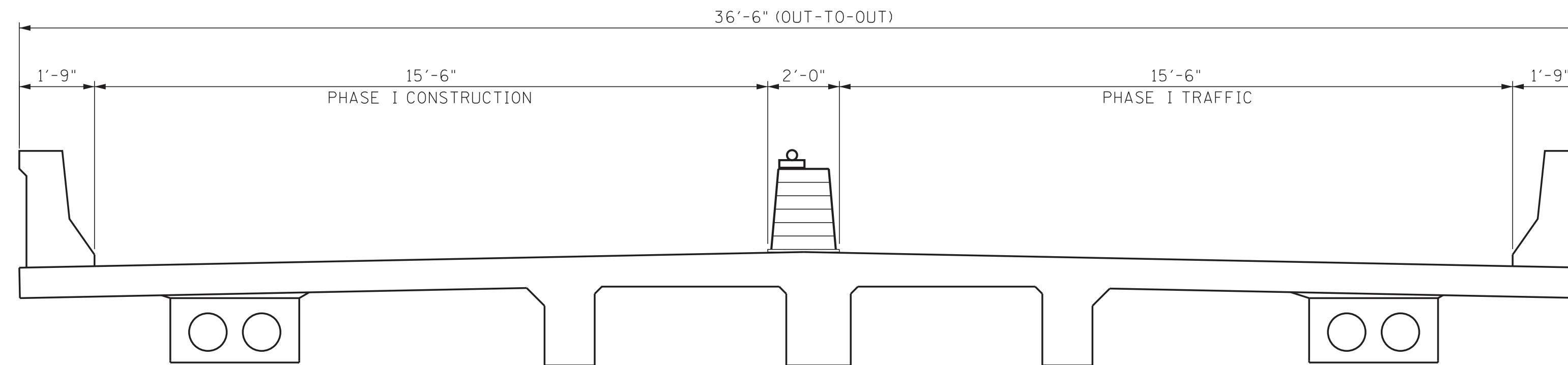
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DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____



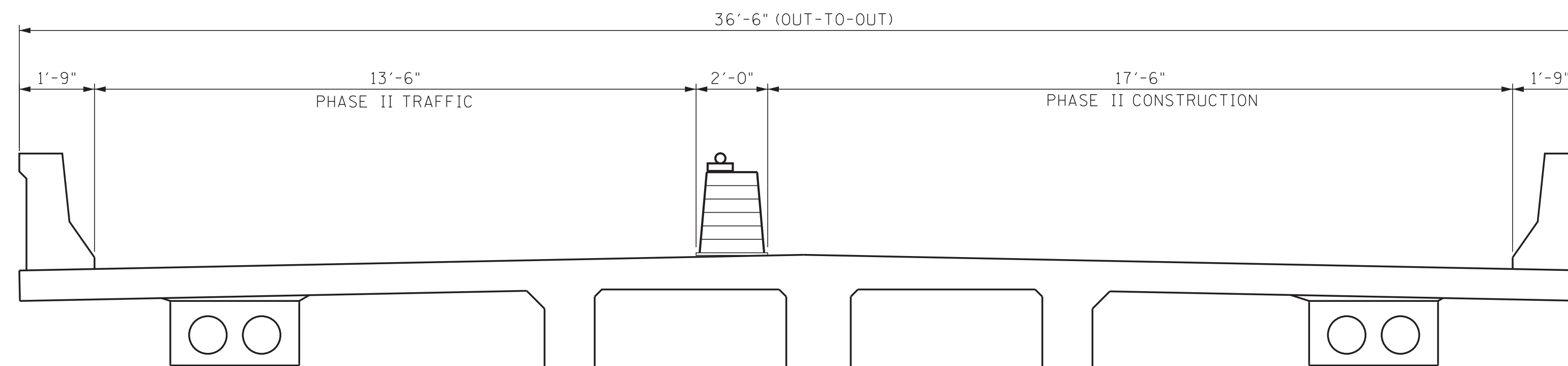
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
79-SR1-26.85L
OVER
CSXT RR
BR. NO. 79SR0010030
SHELBY COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B9

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(79-SR1-26.85L)

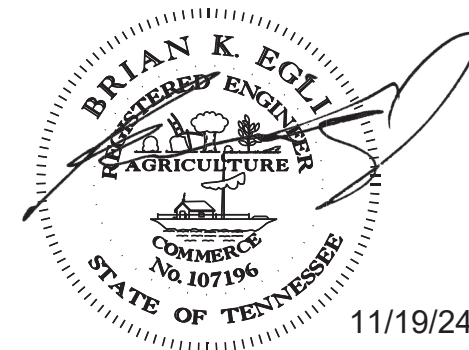
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(79-SR1-26.85L)

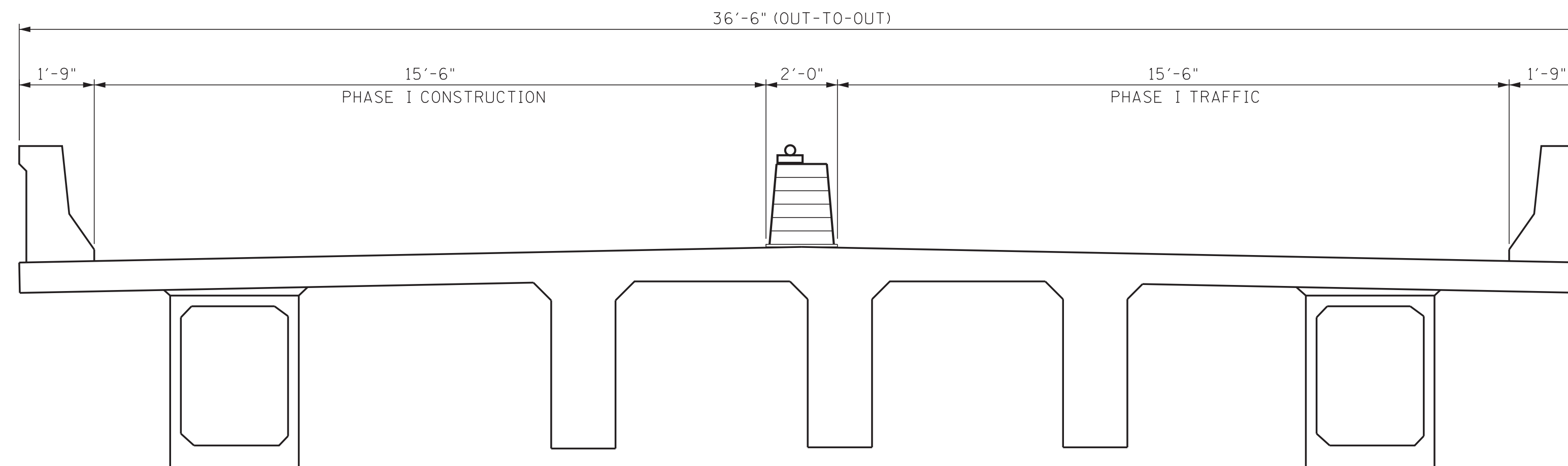
(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
(SPANS 1, 2, 4, & 5)
79-SR1-26.85L
OVER
CSXT RR
BR. NO. 79SR0010030
SHELBY COUNTY
2025

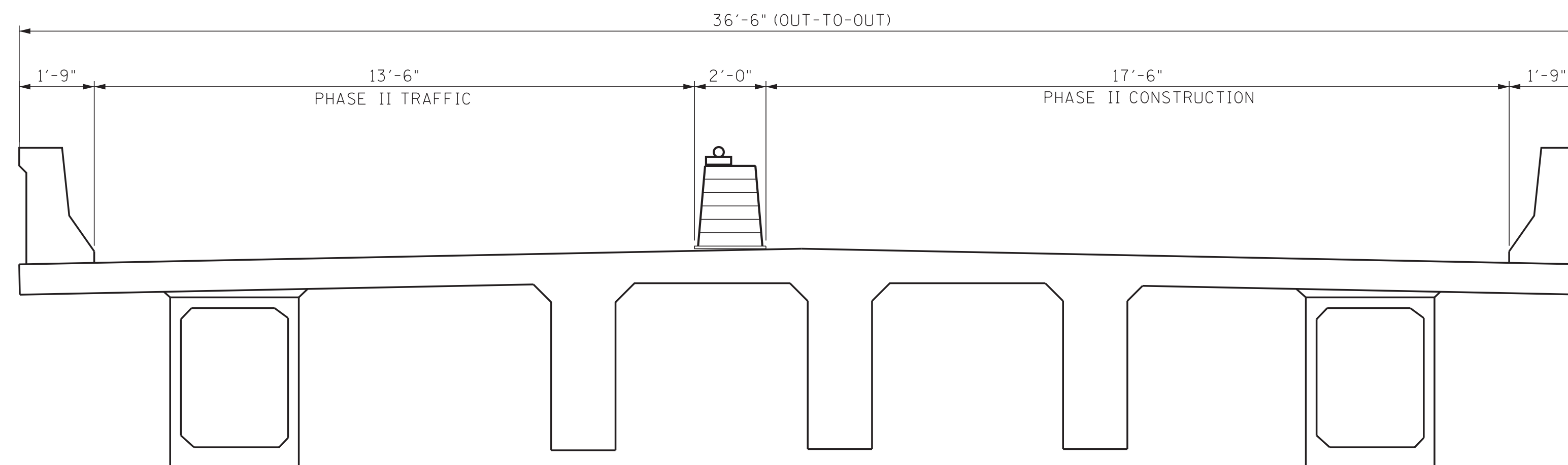
PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B10

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WESTBOUND BRIDGE
(79-SR1-26.85L)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

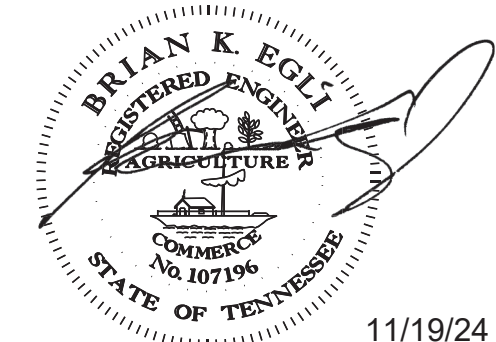


WESTBOUND BRIDGE
(79-SR1-26.85L)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____

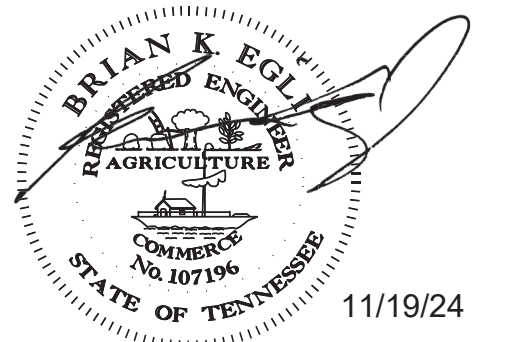


11/19/24

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
(SPAN 3)
79-SR1-26.85L
OVER
CSXT RR
BR. NO. 79SR0010030
SHELBY COUNTY
2025

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DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____



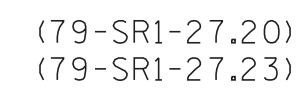
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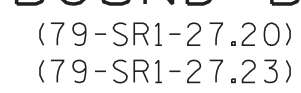
11/19/24

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
79-SR1-27.3
OVER
GULFSTREAM AVE.
BR. NO. 79SR0010049
SHELBY COUNTY
2025

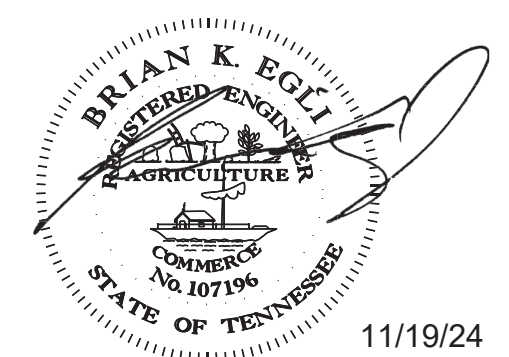


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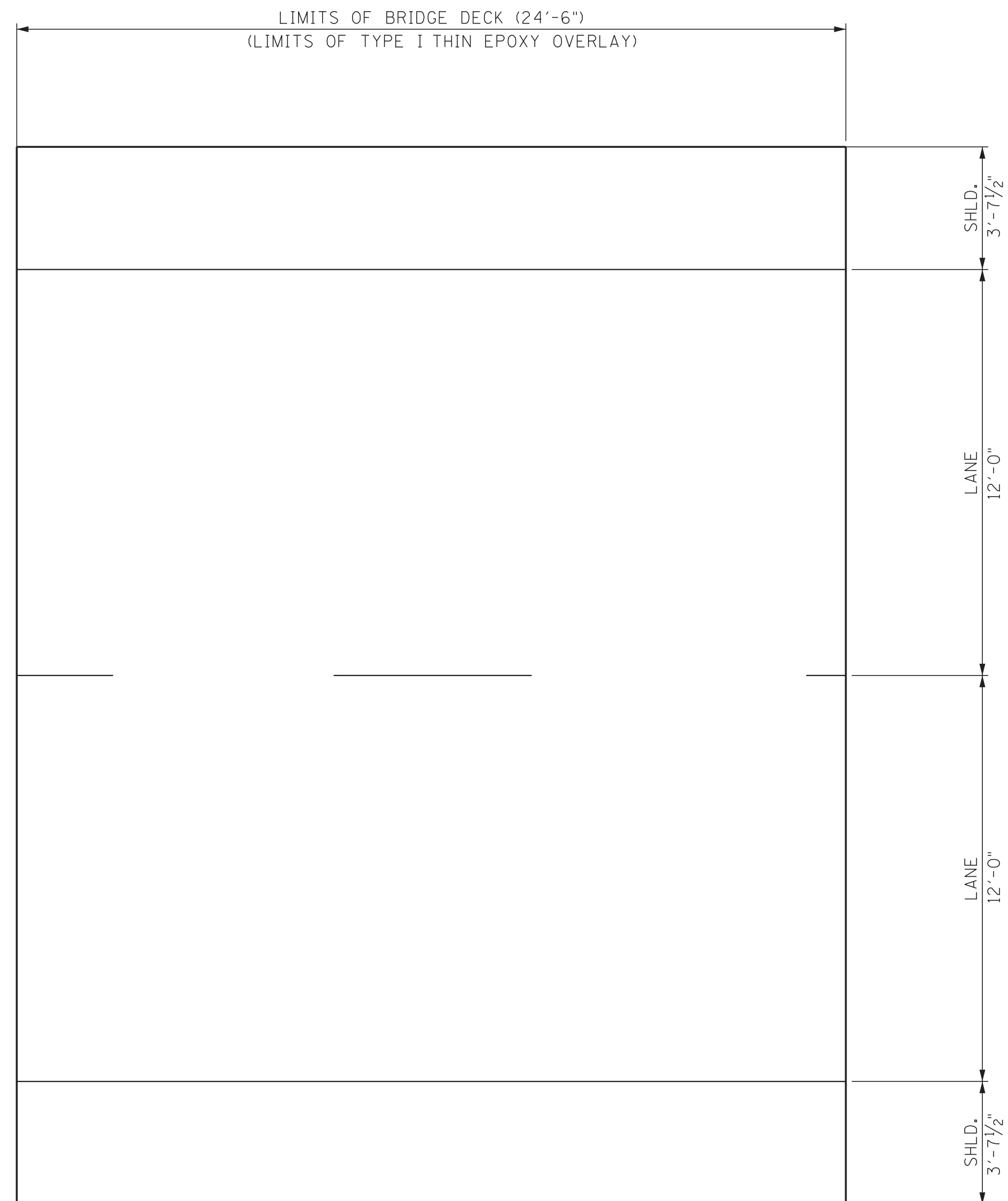
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IN 132518.00

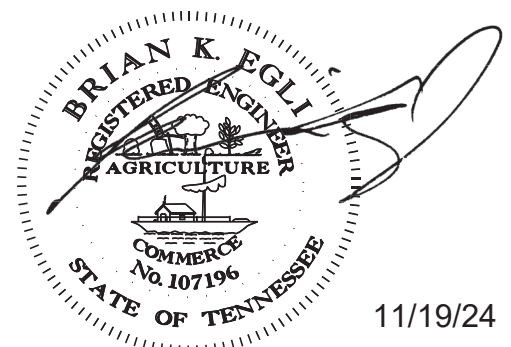
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
79-SR1-27.20
OVER
CSXT RR
79-SR1-27.23
OVER
GULFSTREAM AVE.
BR. NO. 79SR0010047
79SR0010049
SHELBY COUNTY
2025

PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B14

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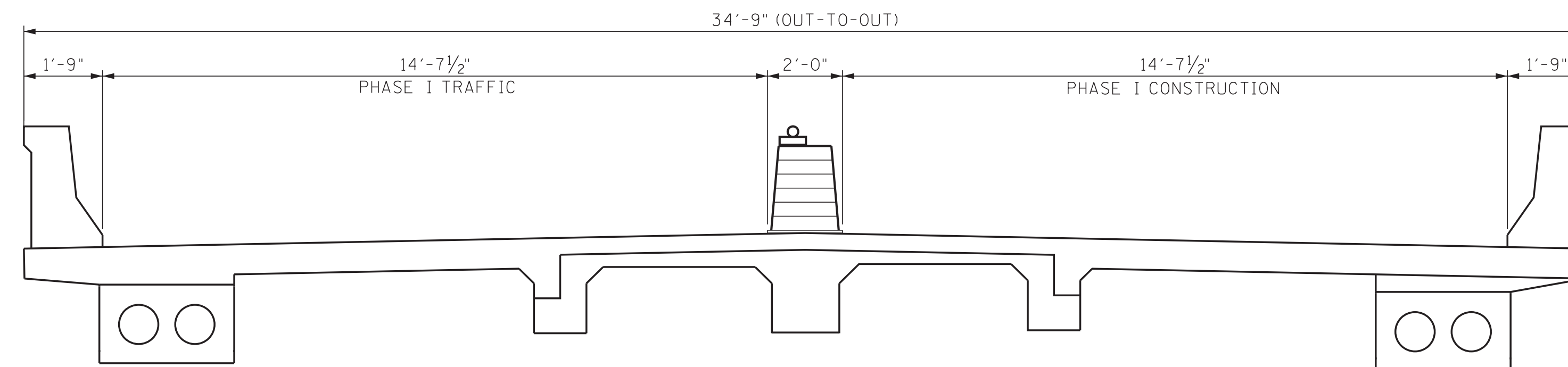
BRIDGE ID. NO. 79SR0010035



PLAN VIEW
79-SR1-29.81
OVER
OVERFLOW
BR. NO. 79SR0010035
SHELBY COUNTY
2025

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____

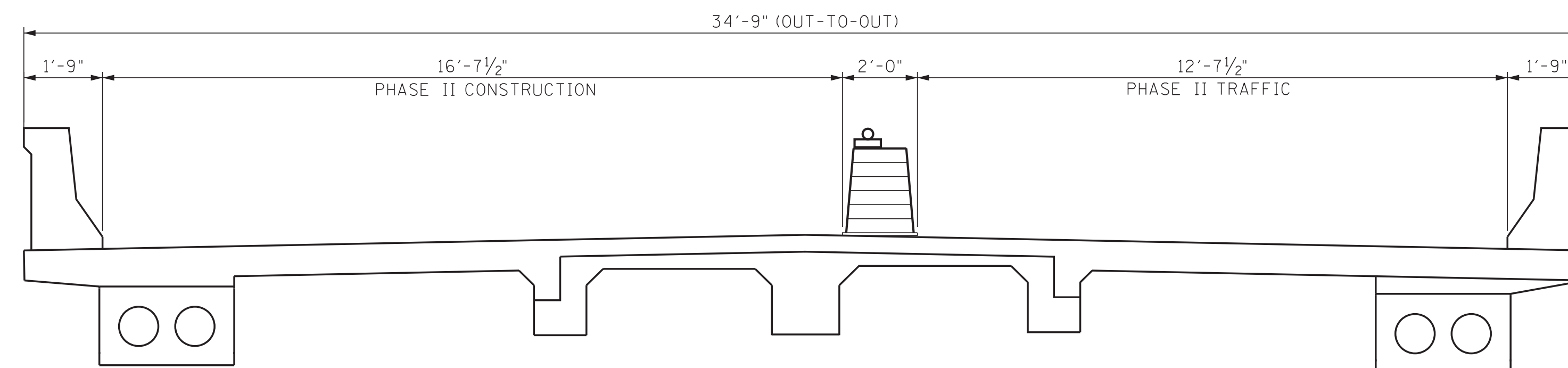
PROJECT NO.	YEAR	SHEET NO.
79S001-M3-011	2025	B15

[illegible]

EASTBOUND BRIDGE
(79-SR1-29.81)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

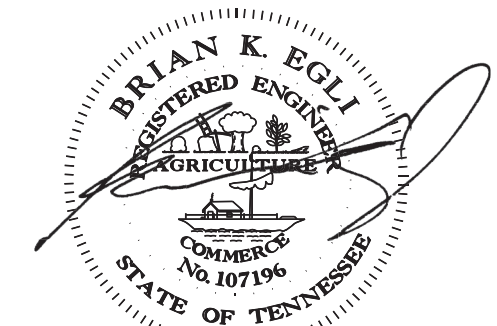


EASTBOUND BRIDGE
(79-SR1-29.81)

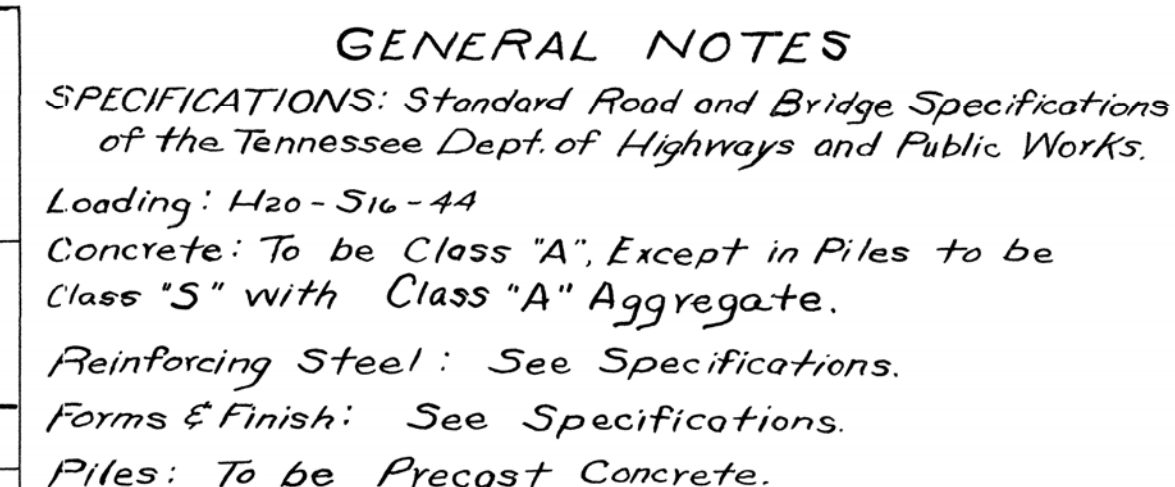
PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

DESIGNED BY _____ DATE _____
 DRAWN BY Z.HAYNES DATE 7/24
 SUPERVISED BY K. MARTINKO DATE 7/24
 CHECKED BY _____ DATE _____



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
79-SR1-29.81
OVER
OVERFLOW
BR. NO. 79SR0010035
SHELBY COUNTY
2025



Handrail (3 Spans @ 50' Clear)	E-12-105
(See Handrail Note this sheet)	
Deck Girders (3 Spans @ 50' Clear)	F-10-52
Skewed Plan - 45° Right	F-10-91
Abutments No. 1 & 2 (Pile Bent Type)	F-14-49
Bents No. 1 & 2 (Two Post Type With Coll Wall)	F-14-50
Piles	A-7-141

Center Span: Build According to Std. Dwg. E-12-105
for 50' Clear, Intermediate Span.

Long Side: Build according to Std. Dwg. E-12-105
Except "M" = 56'-3", "X" = 2", and "L" = 6 @ 7'-5 1/2"
Make 12-1/2" ϕ Bars F = 7'-1" & 24-1/2" ϕ Bars G = 7'-7"

Short Side: Build according to Std. Dwg. E-12-105
for 50' Clear End Span.

22 Roadway Drains, 0'-8" long, required.
Omit Drains over Fill Slope.



If in the opinion of the engineer, the load bearing capacity of the soil (after excavating to foundation level for Bents No. 1 & 2) is determined as a minimum to be 4,000 ^{lbs}/_{sq} ft., No piles will be required for these bents. If soil conditions warrant piles, an untreated timber pile, 40' long, shall be driven in a pit. The pit shall be located on the Surrey E (Sta. 27+50 - Bent No. 1 and Sta. 28+25 - Bent No. 2) and excavated to E1.265.35 (Bent No. 1) - E1.265.13 (Bent No. 2). From results obtained, treated timber piles shall be ordered of such length to obtain a minimum bearing of 15 tons, both bents.

A precast concrete pile, 40' long, shall be driven in the final location of Abutments No. 1 & 2. From the results obtained, precast concrete piles shall be ordered of such length to obtain a minimum bearing of 27 tons, both Abutments.

ESTIMATED QUANTITIES									
ITEM	Dry Excavation Cu. Yds.	Concrete Class "A" Cu. Yds.	Steel - Lbs			Precast Conc Test Piles Lin. Ft.	Precast Conc Piles Size #1 Lin. Ft.	Untreated Timber Test Piles Lin. Ft.	Treated Tim ber Test Piles - Size Lin. Ft.
			Reinforcing	Structural	Forgings				
Superstructure		14.8	2,858						
Deck Girders 3 @ 50' Clear		294.0	67,421	2511	6381				
Substructure									
Abutment No.1		17.9	3,278						
Bent No 1	115	75.3	6,916						
Bent No 2	55	72.9	7,046						
Abutment No.2		17.9	3,278						
Totals	170	492.8	90,797	2511	6381	80	490	80	1400

Note: Cost of Roadway Drains and all Joint Material to be included in unit cost of Class "A" Concrete.

*Rubbing Note: The Contractor's Attention is Especially called to the Special Provision on finishing concrete. This Provision Eliminates Rubbing, Except in the few cases Specified.

Note: If piles are not required for Bents No. 1 & 2, deduct 7.8 Cu.Yds. of Class "A" Concrete from quantities given above for each bent, and 15 cu.yds. dry excavation from quantities shown above for each bent.



28' Roadway with Safety Curbs

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
AND PUBLIC WORKS
NASHVILLE

LAYOUT OF BRIDGE
OVERHEAD CROSSING
L & N RAILROAD
Sta. 27+09.50
SHELBY CO.
1957

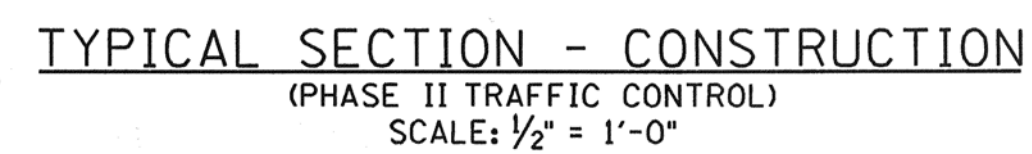
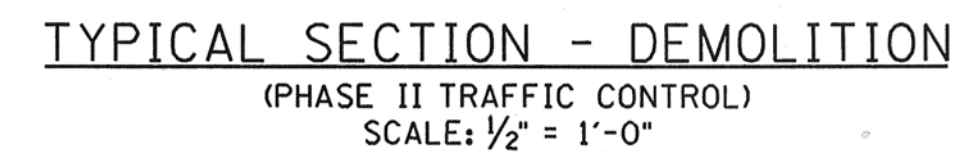
7557

CORRECT. Fred Grove
BRIDGE ENGINEER

APPROVED. Herbert M. Bates
STATE HIGHWAY ENGINEER

F-14-48

DESIGNED BY _____ DATE _____
DRAWN BY H.G. DATE 8-12-57
TRACED BY B.H. DATE 8-15-57
CHECKED BY _____ DATE _____

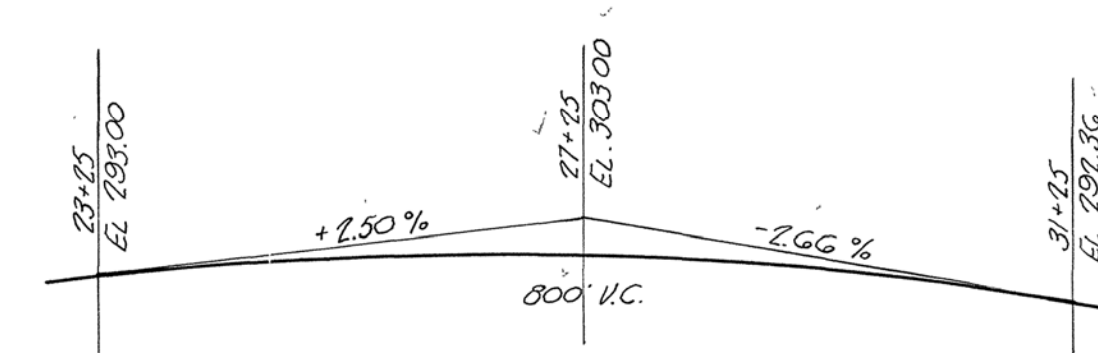


BR-69-59

: /2001/01012412/FINAL/SY-22.DGN 1/27/04

F-001-1 (37)

F-001-1 (37)



ELEVATIONS REFER TO FINISHED GRADE

FOR DETAILS OF HANDRAIL SEE DWG. E-12-105 AND THIS SHEET
FOR DETAILS OF DECK GIRDERS SEE DWG. E-15-122
FOR DETAILS OF ABUTS No. 1 & 2 SEE DWG. E-15-123
FOR DETAILS OF BENTS No. 12, 3 & 4 SEE DWG. E-15-124
FOR DETAILS OF CONC. PILES SEE DWG. E-2-118

THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD TRUCK AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH THE REGULATIONS STIPULATED BY THE L & N RY SO AS TO MAINTAIN CLEARANCE AND NOT TO INTERRUPT TRAFFIC. THE MINIMUM CLEARANCES DURING CONSTRUCTION SHALL BE AS FOLLOWS: HORIZONTAL - 8'-0", VERTICAL - 20'-0".

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE
TENNESSEE DEPARTMENT OF HIGHWAYS AND PUBLIC WORKS.
LOADING: 1120-516-44.
CONCRETE: TO BE CLASS 21 ✓
REINFORCING STEEL SEE SPECIFICATIONS.
FORMS & FINISH: SEE SPECIFICATIONS ✓
PILES: TO BE PRECAST CONCRETE ✓

REQUIRED: 54'-3" x 0'-9" TRANSITE DRAINS OR EQUAL.



BUILD HANDRAIL ACCORDING TO DWG E-12-105 AND THE FOLLOWING DIMENSIONS:

SPANS NO 12, 14, 5R, & 5L. BUILD ACCORDING TO 2G'
CLEAR END JOAN EXCEPT MAKE M=29'-2"; X=2";
L=4@5'-2". LENGTHEN BARS F AND G 2"

SPANS NO. 2R, 4R, 2L, & 4L. BUILD ACCORDING TO 26' ✓
CLEAR INTERMEDIATE SPAN. ✓

SPANS NO. 32 & 31: BUILD ACCORDING TO 30' CLEAR
INTERMEDIATE SPAN EXCEPT MAKE N-S 1'-0", L-6'-6"-9",
SHORTEN BARS F AND G 4"

BEFORE ANY PILES ARE ORDERED, A PRECAST CONCRETE TEST PILE, 40' LONG, SHALL BE DRIVEN IN ITS FINAL LOCATION IN ONE EXTENSION OF EACH ABUTMENT AND BENTS 15-4. FROM THE RESULTS OBTAINED, PILES SHALL BE ORDERED OF SUCH LENGTH AS TO PROVIDE A MINIMUM BEARING OF 18 TONS FOR THE ABUTMENTS AND 23 TONS FOR BENTS 15-4.



SHOWING EXPANSION DETAILS AT BENTS

ESTIMATED QUANTITIES					
ITEM	CONCRETE CLASS A	REINFORCING STEEL	STRUCTURAL STEEL	PRECAST CONC. PILES SIZE 1 *	CONCRETE TEST PILES
	CU YDS	LBS	LBS	LIN FT	
SUPERSTRUCTURE					
HANDRAIL	16.5	3163			
DECK GIRDERS	139.2	23,188	380		
SUBSTRUCTURE					
ABUTMENT NO. 1	12.4	1060			
PENT NO. 1	6.2	403			
PENT NO. 2	38.1	2317			
PENT NO. 3	38.0	2317			
PENT NO. 4	6.2	391			
ABUTMENT NO. 2	12.5	1060			
TOTALS	257.1	33,899	380	480	160

NOTE: COST OF ALL EXCAVATION SHALL BE INCLUDED IN THE ITEMS BID ON

NOTE: BITUMINOUS SURFACING IS INCLUDED IN THE ROADWAY QUANTITIES.

NOTE: THE COST OF REMOVAL OF PRESENT HANDRAIL, PARTS OF PRESENT CURBS, PARTS OF PRESENT ABUTMENTS AND BENTS AS INDICATED, THE COST OF DRAINS, JOINT MATERIAL, AND JOHN-MANVILLE PACKING, TO BE INCLUDED IN THE UNIT PRICE BID FOR CLASS A CONCRETE.

* IF IT BECOMES NECESSARY TO USE SIZE 2 PILES, THE CONTRACTOR WILL BE ALLOWED AN INCREASE IN HIS SIZE 1 BID PRICE OF 25 %

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
AND PUBLIC WORKS
NASHVILLE

OVER L&N RY.
STA. 26+25.0
SHELBY COUNTY
1957

CORRECT Fred Greve
BRIDGE ENGINEER

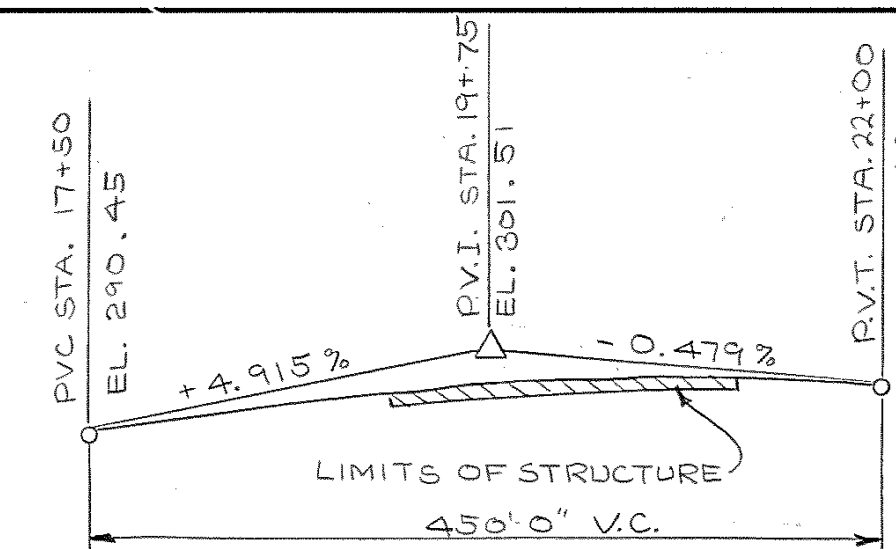
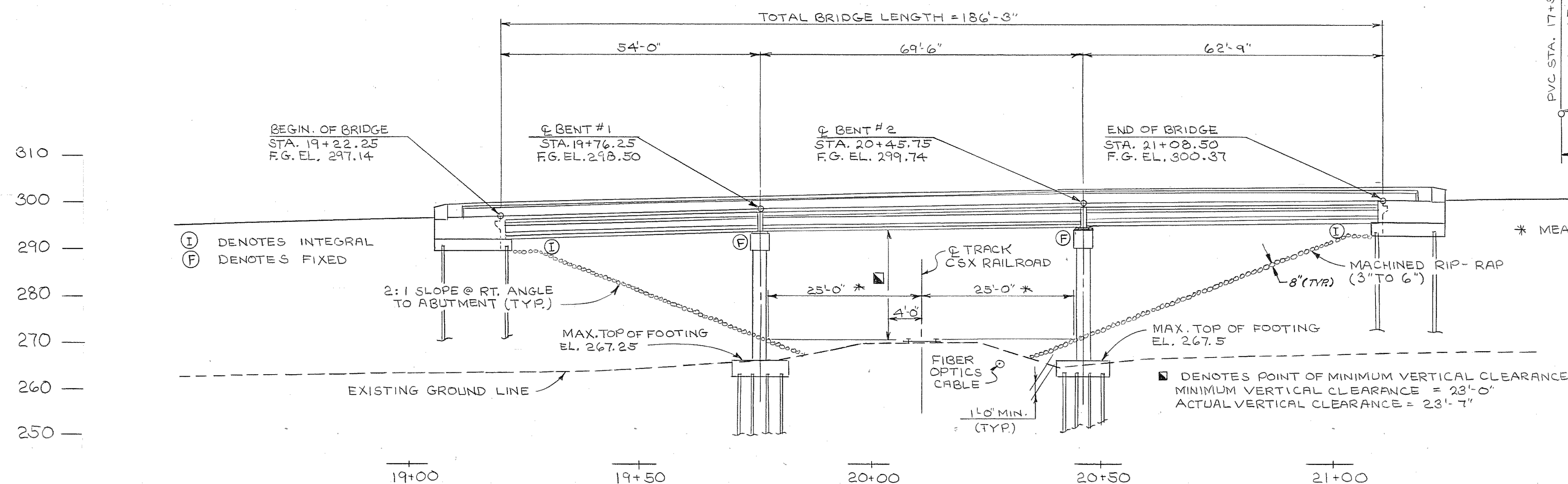
APPROVED Harold McVates
STATE HIGHWAY ENGINEER

F-15-121

CONST. No. 79187-3220-14

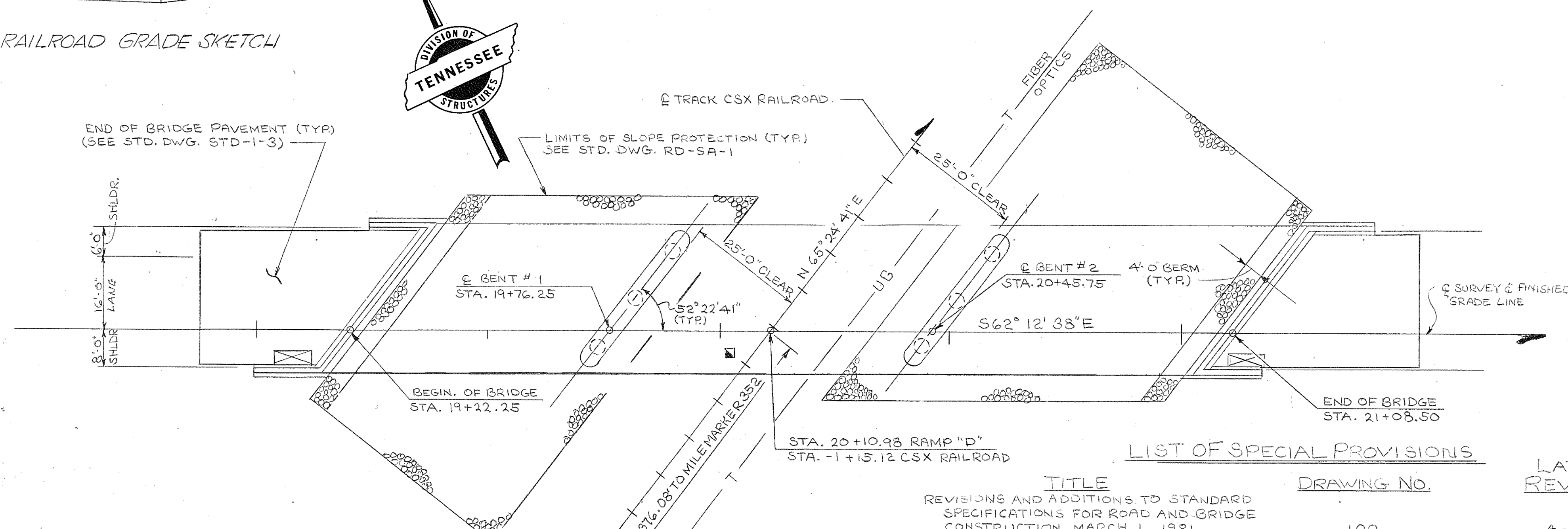
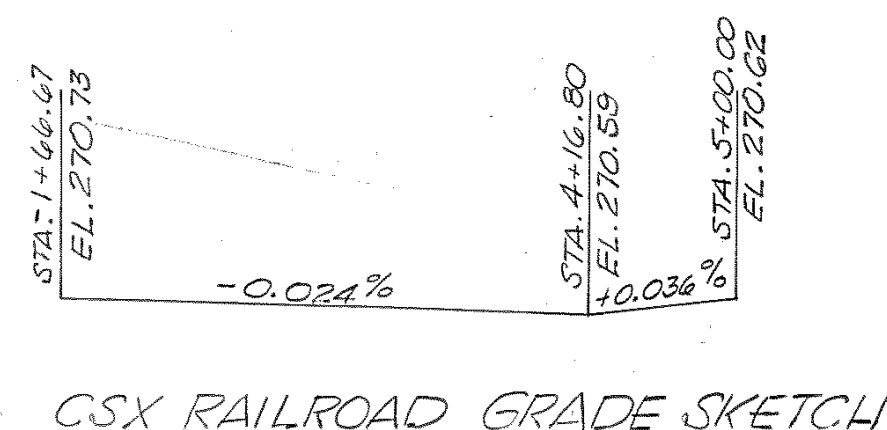
PROJECT NO.	YEAR	SHEET NO.
STP-F-385(9)	1994	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	12-13-93	mmf	REMOVED NOTE & REVISED DATES
2	3-23-94	mmf	REVISED GUARDRAIL ATTACHMENT & SPECIAL PROVISION DATES
3	4-12-94	PAM	GUARDRAIL NOTES, REVISED SPECIAL PROV. & STD. DWG. DATES
4	6-9-94	PAM	CORRECTED TITLE & ADDED *S



* MEASURED AT RIGHT ANGLE TO TRACKS.

ELEVATION



- DENOTES: POINT OF MINIMUM VERTICAL CLEARANCE.
 □ DENOTES: END OF BRIDGE DRAINS - SEE STANDARD DWGS STD-1-4, 5 & 6.

PLAN

LIST OF SPECIAL PROVISIONS

TITLE	DRAWING NO.	LATEST REV. DATE
REVISIONS AND ADDITIONS TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, MARCH 1, 1981	100	4-11-94
CONCRETE STRUCTURES	604	10-25-93
CONTRACTOR - MIX, DESIGN AND TESTING		
STRUCTURAL CONCRETE	604CX	2-14-94
PRECAST PRESTRESSED BRIDGE DECK PANELS	604P	10-25-93
RIDEABILITY OF BRIDGE DECKS AND ROADWAY APPROACHES	604R	3-30-92
PRECAST PRESTRESSED CONCRETE BRIDGE MEMBERS	615	12-16-91
REGARDING APPROVAL OF SHOP DRAWINGS	105A	4-5-93
EPOXY COATED REINFORCING STEEL	907A	2-15-93

NOTE: MACHINED RIP-RAP TO BE 3" TO 6" AND BE PAID FOR UNDER ITEM 709-05.04

TITLE	DRAWING NO.	LATEST REV. DATE
△ LAYOUT	M-288-160	6-16-94
△ GENERAL NOTES AND ESTIMATED QUANTITIES	M-288-161	4-12-94
FOUNDATION DATA	M-288-161A	
SUPERSTRUCTURE	M-288-162	12-13-93
SUPERSTRUCTURE DETAILS	M-288-163	
TYPE III PRESTRESSED I-BEAM DETAILS (SPAN 1)	M-288-164	10-26-94
TYPE II PRESTRESSED I-BEAM DETAILS (SPAN 2)	M-288-165	
TYPE III PRESTRESSED I-BEAM DETAILS (SPAN 3)	M-288-166	10-26-94
ABUTMENT NO. 1	M-288-167	
ABUTMENT NO. 2	M-288-168	
ABUTMENTS NO. 1 AND 2 DETAILS	M-288-169	12-13-93
BENTS NO. 1 AND 2	M-288-170	
FINAL FOUNDATION DATA	M-288-171	
BILL OF STEEL	M-288-172	12-13-93

TITLE	DRAWING NO.	LATEST REV. DATE
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△ BRIDGE RAILING CONCRETE PARAPET	STD-1-1	3-28-94*
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	3-28-94*
BRIDGE END DRAIN W/ P&B	STD-1-6	3-28-94*
BRIDGE END DRAIN W/P&B	STD-1-7	3-28-94*
BRIDGE END DRAIN 4'X8" W/P&B	STD-1-9	3-28-94*
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	10-25-93*
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	10-25-93*
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DET.	STD-4-3	2-15-93
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DET.	STD-4-4	10-25-93*
STANDARD PILE DETAILS	STD-5-1	10-25-93*
STANDARD PILE DETAILS	STD-5-2	10-26-92
STANDARD SEISMIC DETAILS	STD-6-1	2-15-93
STANDARD SEISMIC DETAILS	STD-6-2	3-28-94*
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB	STD-9-1	9-01-91
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1	5-11-92
SLOPE PROTECTION	RD-SA-1	4-29-88

* DENOTES: THESE DRAWINGS TO BE PRINTED WITH PLANS.

30'-0" ROADWAY WITH STD-1-1 BRIDGE RAIL
2012 ADT = 1700 DESIGN SPEED = 50 M.P.H.

BRIDGE NO. 8

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

LAYOUT

STATE ROUTE-385 - RAMP "D"
 OVER CSX RAILROAD
 BRIDGE ID NO. 79SR0010047
 STATION 20+15.00
 SHELBY COUNTY
 1994

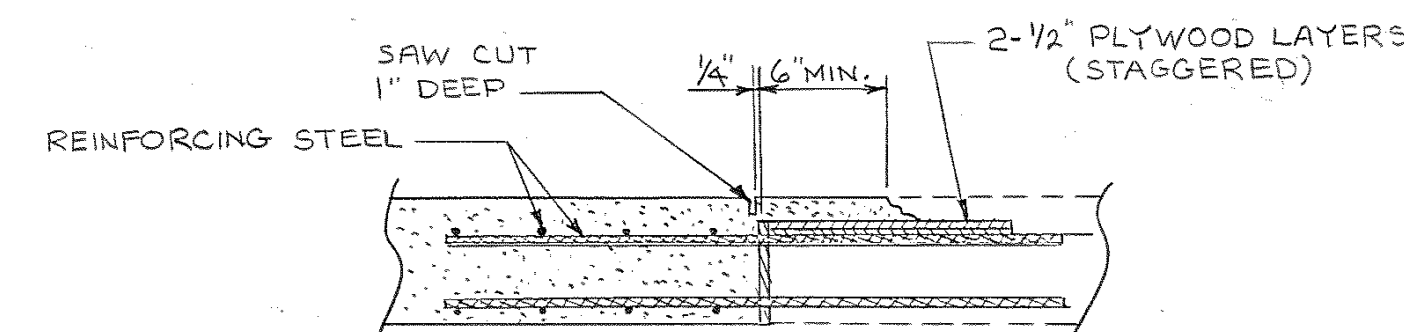
DESIGNED BY MOLLIE FLOYD DATE 12/92
 DRAWN BY M.R. MANNICHEN DATE 4/93
 SUPERVISED BY RLH/RBP DATE 4/93
 CHECKED BY Mollie Floyd DATE 8-93

CORRECT *Edward P. Moseman*
ENGINEER OF STRUCTURES

M-288-160

PROJECT NO.	YEAR	SHEET NO.
STP-F-385(9)	1994	

REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
1	12-13-92	MMF	REVISED PARAPET DETAIL



SLAB CONSTRUCTION JOINT

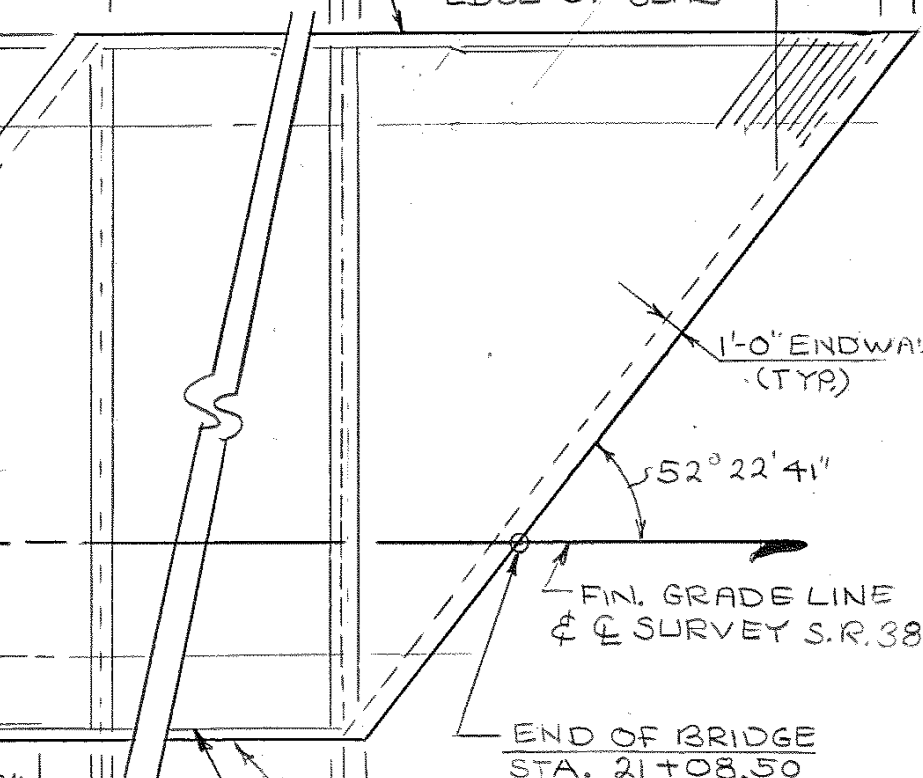
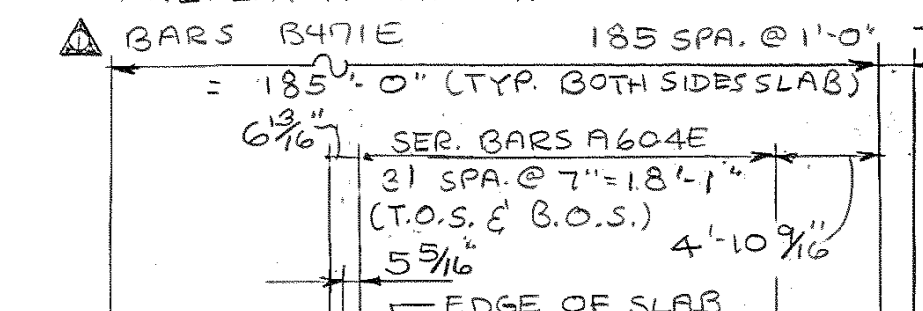
NOTE: ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT SHOWN ABOVE.

SLAB POURING SEQUENCE NOTE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION EXCEPT NO JOINT MAY BE LOCATED CLOSER THAN 1/5 SPAN LENGTH FROM AN INTERIOR SUPPORT. THE CONTRACTOR SHALL MAKE ADEQUATE PROVISIONS DURING PLACEMENT OF SLAB TO PREVENT EXTERIOR BEAMS FROM TWISTING. NO EQUIPMENT SHALL BE PERMITTED ON THE BRIDGE UNTIL ALL POURS ARE MADE AND THE CONCRETE IS PROPERLY CURED.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPETS. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. SEE ALSO DWS. NO. STD-1-1.

NOTE: ALL GIRDERS SHALL BE SUPPORTED DURING CONSTRUCTION OF SLAB TO PREVENT ROTATION.



B.O.S. DENOTES BOTTOM OF SLAB
T.O.S. DENOTES TOP OF SLAB

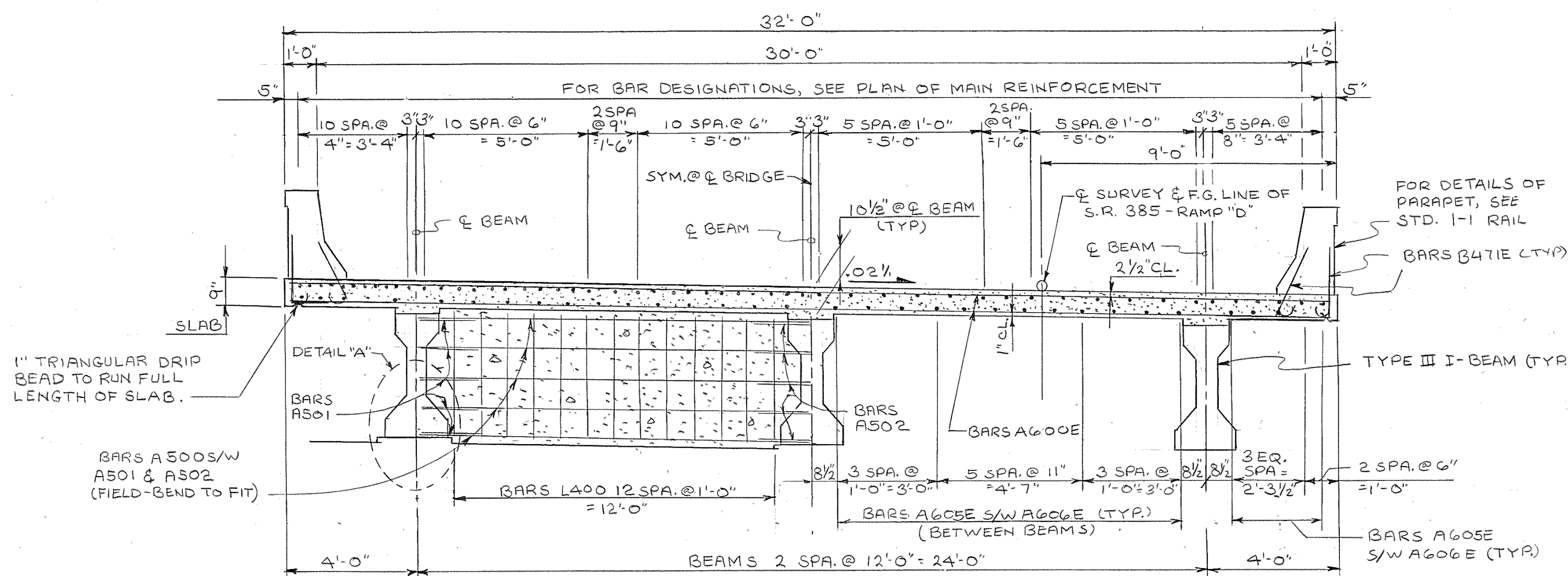
SLAB PLAN

ESTIMATED QUANTITIES

CLASS "A" CONCRETE (BRIDGES) C.Y.	CLASS "D" CONCRETE (BRIDGE DECKS) C.Y.	STEEL BAR REINFORCEMENT LBS.	EPOXY COATED REINFORCEMENT LBS.
25	178	3,308	59,369

BRIDGE NO. 8
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
STATE ROUTE 385-RAMP "D"
OVER CSX RAILROAD
STATION 20+15.00
SHELBY COUNTY
1994

CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

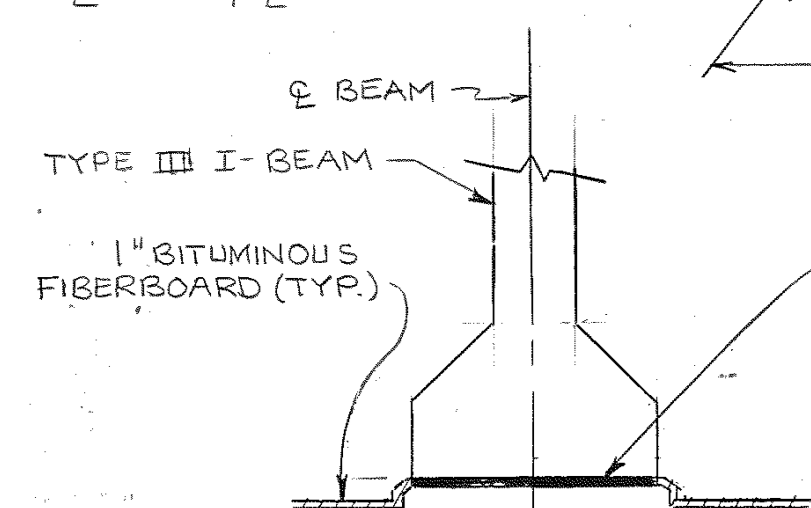
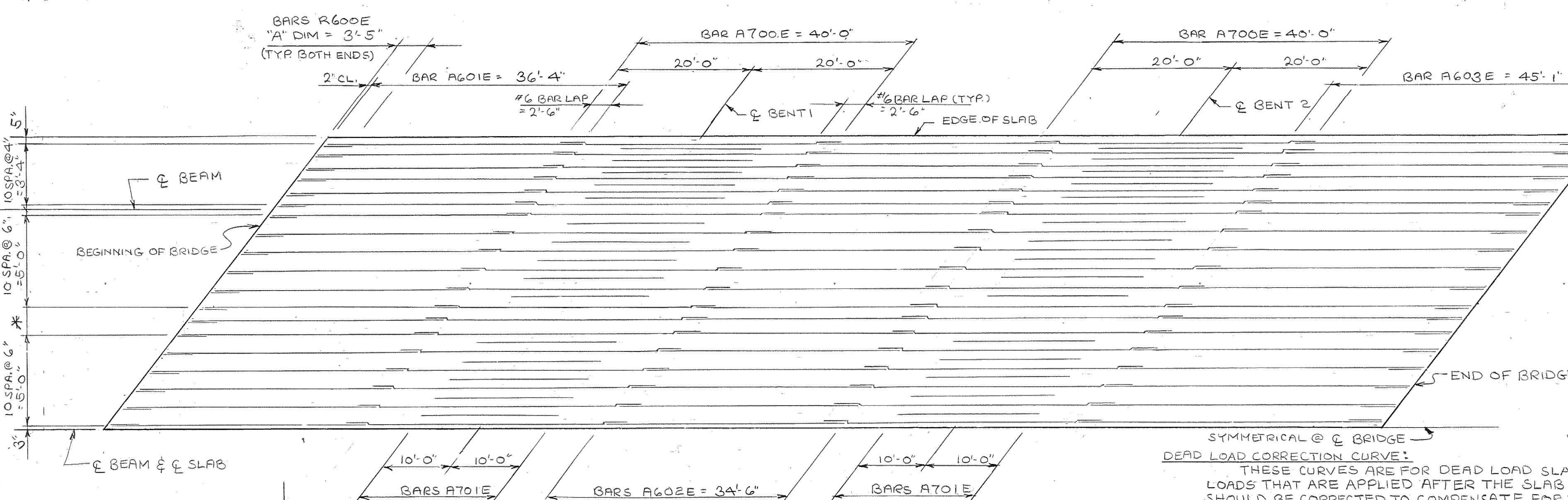


TYPICAL AT SUPPORT

TYPICAL AT MID-SPAN

TYPICAL CROSS-SECTION
(LOOKING FORWARD ON SURVEY)

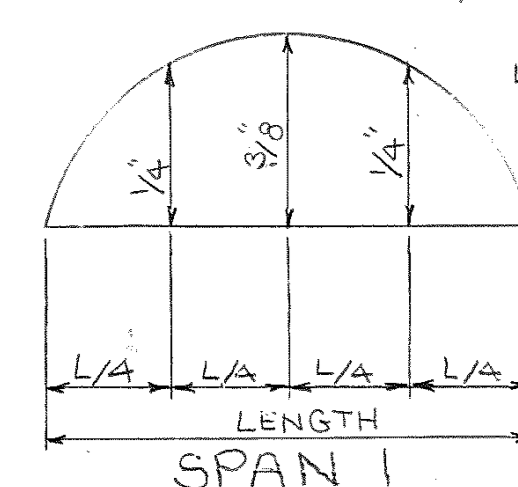
* DENOTES: 2 SPA. @ 9" = 1'-6"



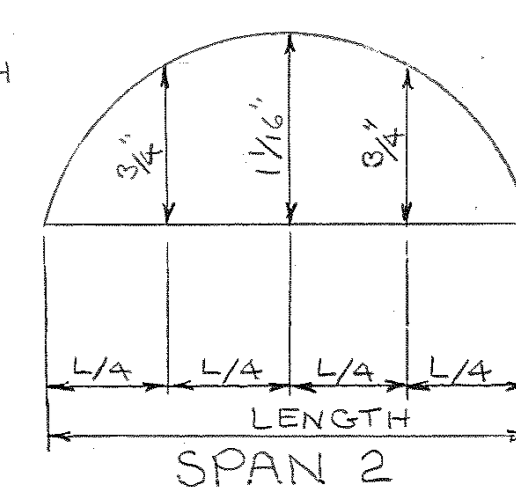
DETAIL "A"

HALF-PLAN OF MAIN REINFORCEMENT

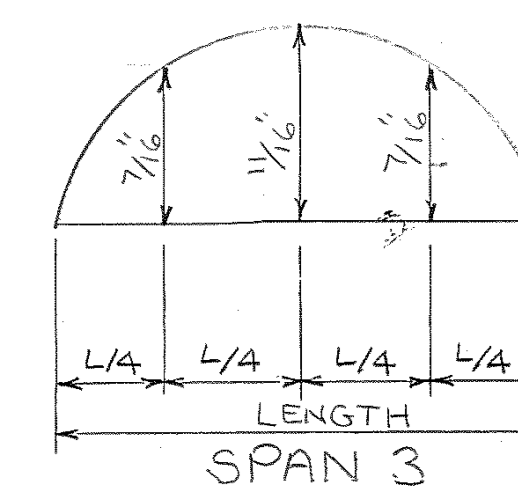
SYMMETRICAL @ C BRIDGE
DEAD LOAD CORRECTION CURVE:
THESE CURVES ARE FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES BY 25%



SPAN 1



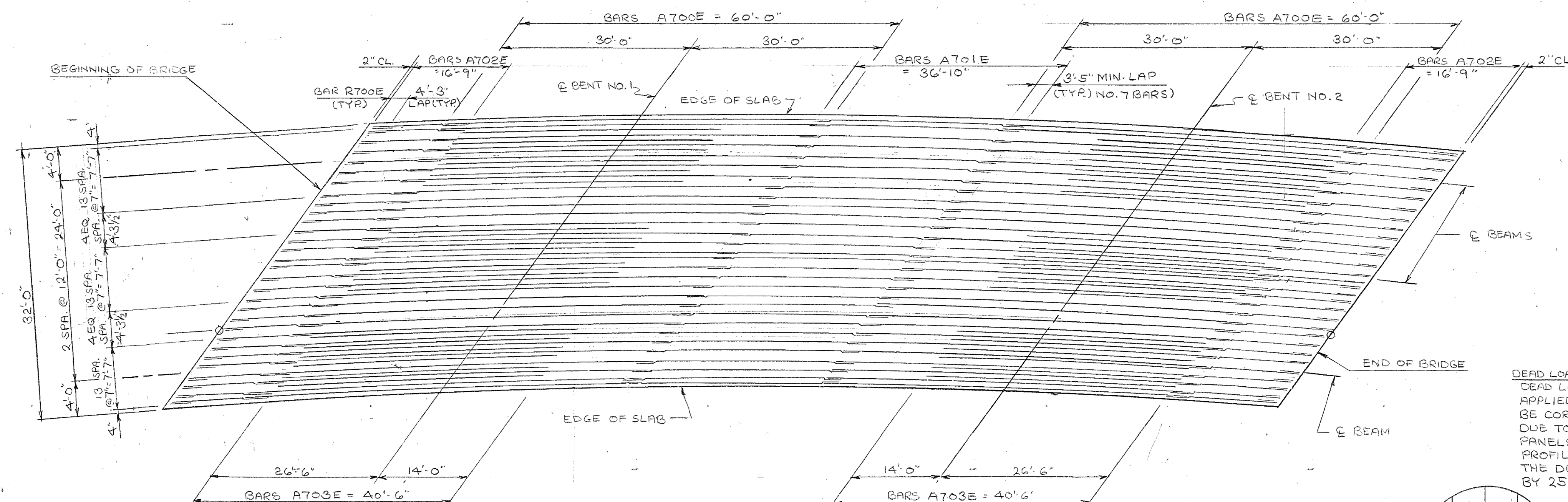
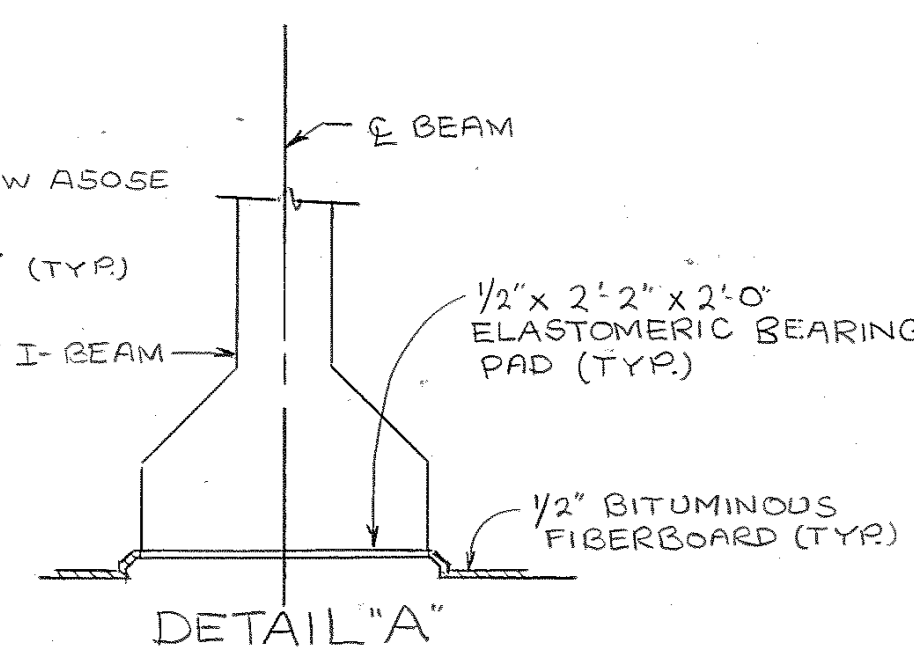
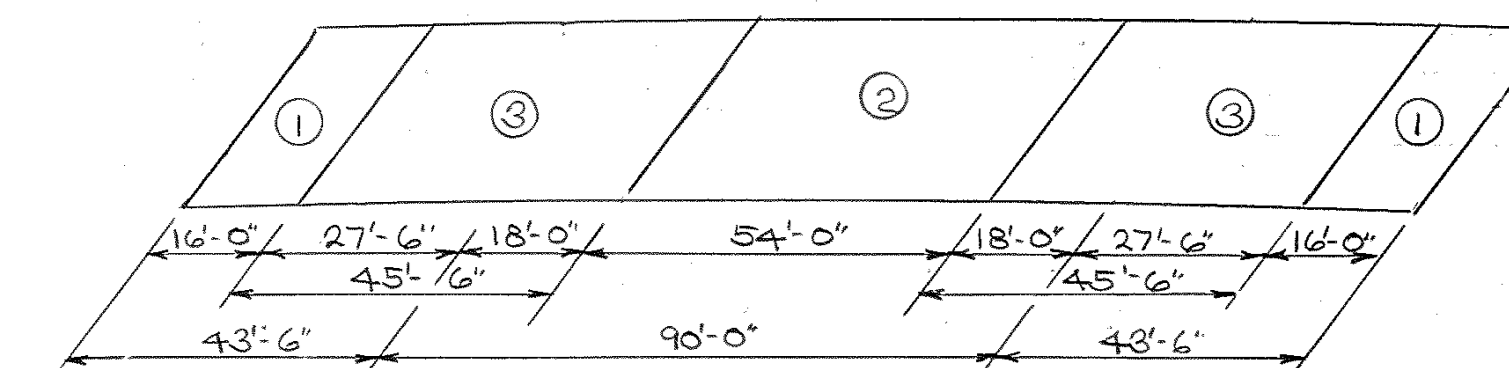
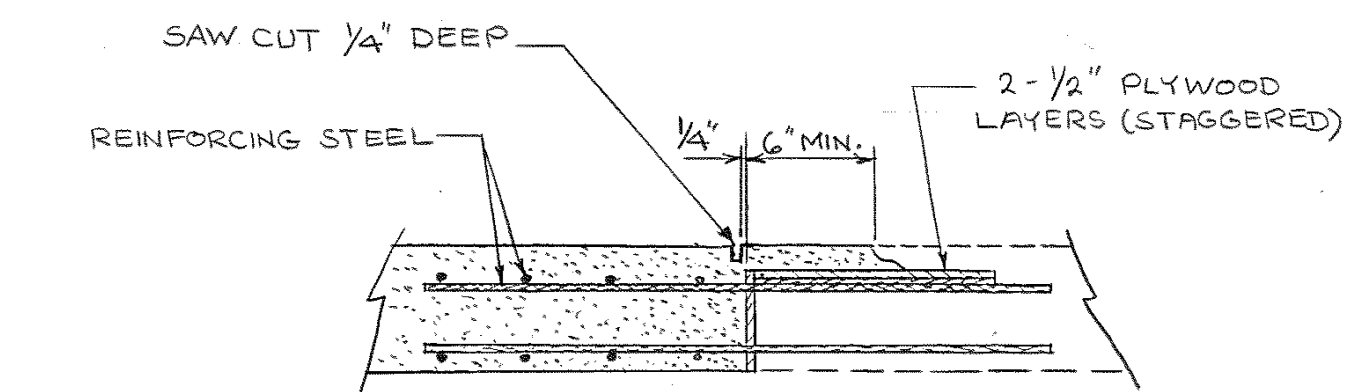
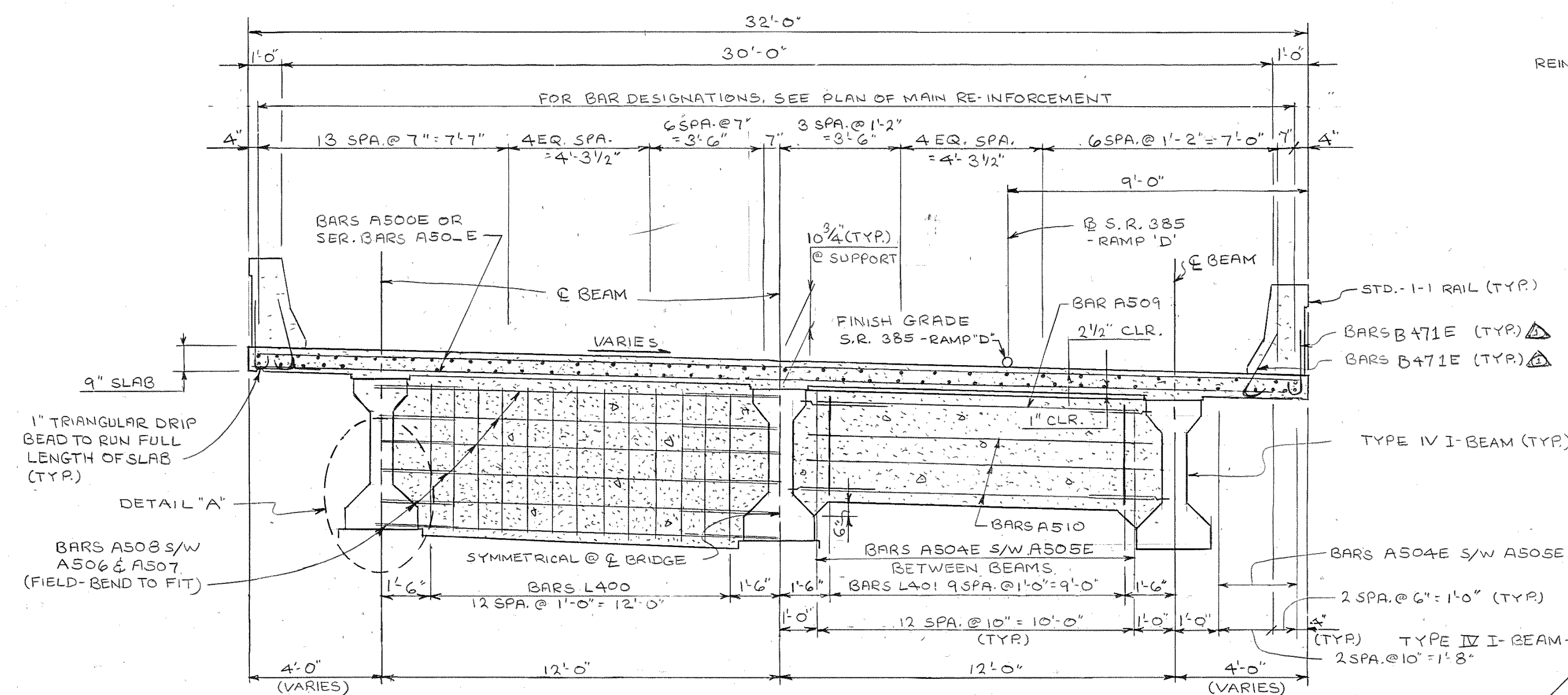
SPAN 2



SPAN 3

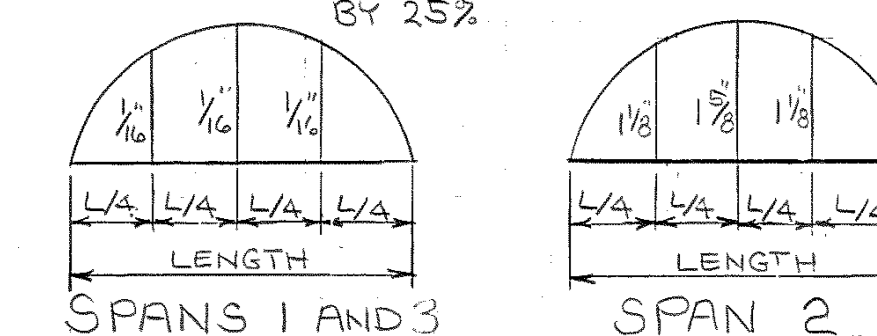
DEAD LOAD CORRECTION CURVE

DESIGNED BY MOLLIE FLOYD DATE 12/92
DRAWN BY M. R. MANNICHEN DATE 3/93
SUPERVISED BY RLH/RAP DATE 3/93
CHECKED BY MOLLIE FLOYD DATE 3/93

[illegible]

ESTIMATED QUANTITIES			
CLASS "A" CONCRETE BRIDGES (C.Y.)	CLASS "D" CONCRETE BRIDGE DECKS (C.Y.)	STEEL BAR REINFORCEMENT (LBS.)	EPOXY COATED REINFORCEMENT STEEL (LBS.)
35	175	4,275	46,071.4

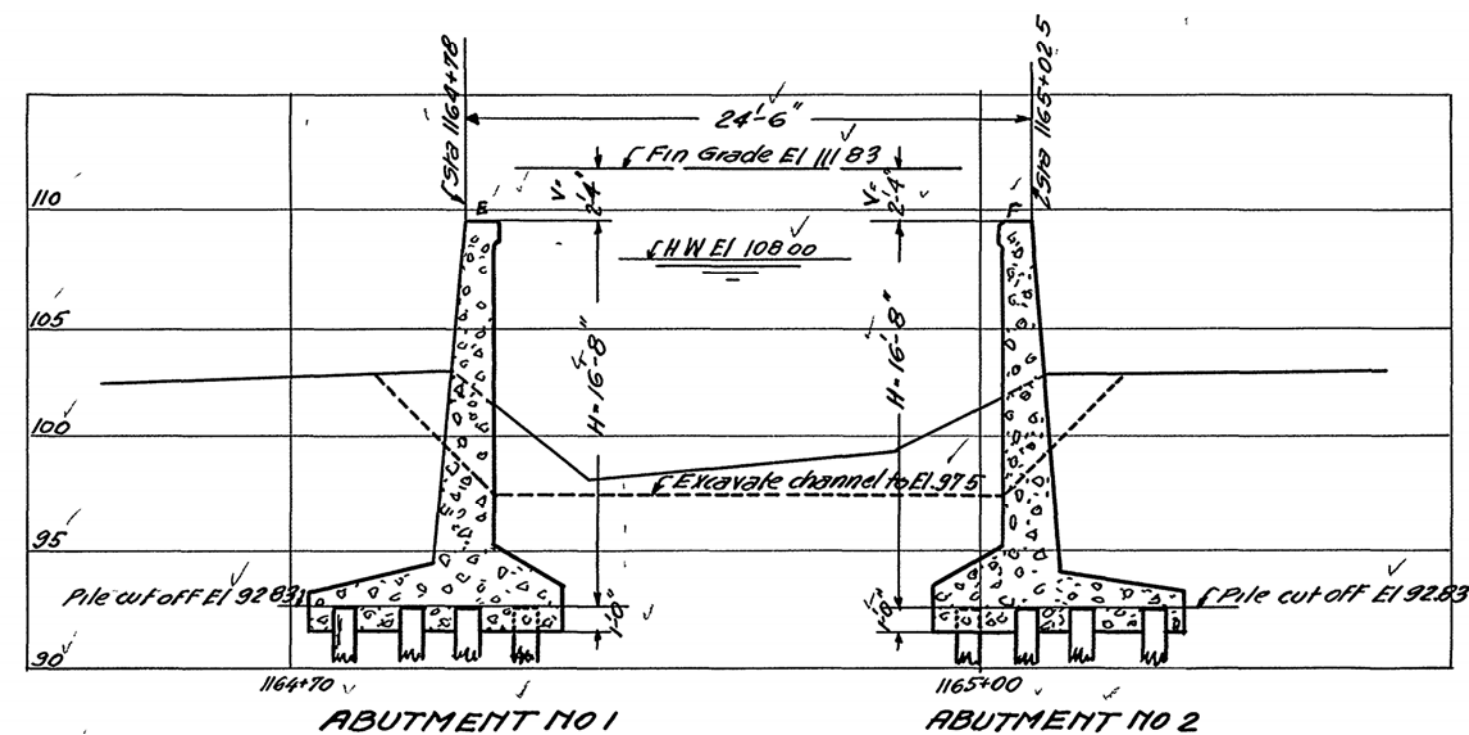
DEAD LOAD CORRECTION CURVE: THESE CURVES ARE FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.



BRIDGE No. 9
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE
STATE ROUTE 385 RAMP "D"
OVER GULFSTREAM ROAD
STATION 27+35.77
SHELBY COUNTY
1994

DESIGNED BY KIP MAYTON DATE 5/93
DRAWN BY M. R. MANNCHEN DATE 6/93
SUPERVISED BY RLH/JAP DATE 6/93
CHECKED BY KIP MAYTON DATE 9/93

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES



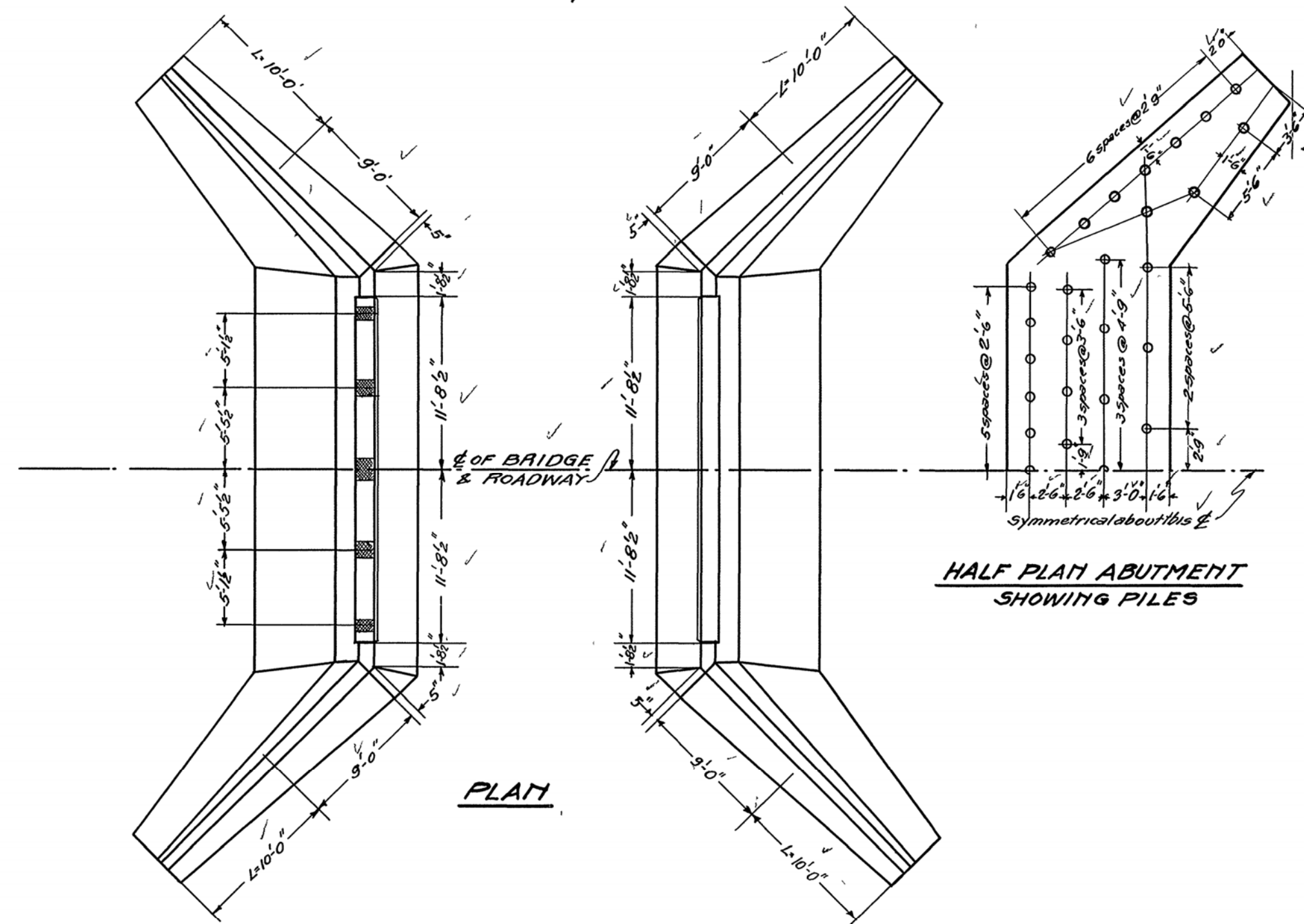
Wale Area
 $10.5 \times 22 = 231.0'$

NOTE
 E EXPANSION END
 F FIXED END

NOTES:
 FOR DETAIL OF HANDRAIL SEE DRAWING D-4-95
 FOR DETAIL OF GIRDER SEE DRAWING D-4-75
 STANDARD CONCRETE DECK GIRDER SPAN 22'-0"
 ROADWAY 20'-0"
 FOR DETAIL OF ABUTMENT NO. 1 & NO. 2 SEE
 DRAWING D-4-96
 STANDARD CONCRETE ABUT-
 MENT TOTAL HEIGHT 19'-0", V-2'-4" H-16'-8" L-10'-0"
 ROADWAY 20'-0"
 SURFACING TO BE 2" OF 1'-2'-4" CONCRETE POURED
 MONOLITHIC WITH GIRDER SLAB

ITEM	ESTIMATED QUANTITIES					
	EXCAVATION		CONCRETE		REINFORCING STEEL	
	CU YDS	CU YDS	CU YDS	CU YDS	STEEL	STEEL
SUPERSTRUCTURE	—	—	1'-2'-3"	1'-2'-4"	LBS	LBS
ABUTMENT NO. 1	104	140	—	—	29.53	5790
ABUTMENT NO. 2	104	140	—	—	29.53	5790
TOTALS	208	280	3.81	222.45	15630	338

Channel excavation to be made by the Road Contractor



HALF PLAN ABUTMENT
 SHOWING PILES

GENERAL NOTES:
 SPECIFICATIONS STANDARD BRIDGE SPECIFICATIONS OF
 THE TENNESSEE DEPARTMENT OF HIGHWAYS
 PILING SHALL BE 16'-0" LONG & UNTREATED.
 THE 1'-0" CONCRETE FOR ENCASEING PILES UNDER ABUTMENTS
 TO BE 1'-2'-4" MIXTURE, COARSE AGGREGATE 1/2" TO 1 1/4"

DESIGNED BY
 DRAWN BY
 TRACED BY
 CHECKED BY

DATE 9-29-25
 DATE 11-10-25
 DATE 9-29-25

STATE OF TENNESSEE
 DEPARTMENT OF HIGHWAYS
 AND PUBLIC WORKS
 NASHVILLE

LAYOUT OF BRIDGE

AT
 STA. 1164+78
 SHELBY CO.
 1925

CORRECT
 L. W. Erickson
 BRIDGE ENGINEER
 APPROVED
 ASST. STATE HIGHWAY ENGINEER

